

LOCAL MEMBER, AM & MP OBJECTION & PETITION

COMMITTEE DATE: 07/02/2018

APPLICATION No. **17/02464/MJR** APPLICATION DATE: 22/09/2017

ED: **HEATH**

APP: TYPE: Full Planning Permission

APPLICANT: Wates Residential

LOCATION: FORMER HIGHFIELDS CENTRE FOR THE ELDERLY AND PHYSICALLY DISABLED, 18 ALLENSBANK ROAD, HEATH, CARDIFF

PROPOSAL: 42 DWELLINGS COMPRISING 12 X TWO BEDROOM HOUSES AND 20 X TWO BEDROOM APARTMENTS FOR OPEN MARKET AND 10 X AFFORDABLE APARTMENTS (5 X ONE BEDROOM AND 5 X TWO BEDROOM), CAR AND CYCLE PARKING AND ASSOCIATED LANDSCAPING.

RECOMMENDATION 1: That planning permission be **GRANTED** subject to the applicant submitting, within six months, to a **SECTION 106** Agreement that units 33-42 (inc) be available for council accommodation and subject to the following conditions:-

1. C01 Statutory Time Limit
2. This approval is in respect of the following plans and documents, unless otherwise amended by any other condition attached to this consent:
 - 3511_PA_001 Site Location Plan
 - 3511_PA_003E Site Layout Plan
 - 3511_PA_004E Coloured Site Layout Plan
 - 3511_PA_005D Management Plan
 - 3511_PA_010C Apartment General Arrangements
 - 3511_PA_011C Apartment Elevations
 - 3511_PA_015B House General Arrangements and Elevations
 - 3511_PA_020C Elevations and Sections 1
 - 3511_PA_021C Elevations and Sections 2
 - 3511_PA_022 Elevations and Sections 3
 - 3511_PA_030C View from Allensbank Road
 - 3511_PA_031A Views of Apartments
 - 3511_PA_032 View from Heathmead
 - 3511_PA_035A Boundary Details
 - 3511_PA_040A Schedule of Accommodation
 - 3511_PA_050 Coloured ElevationsDesign and Access Statement, dated July 2017 (Rev B)
Cambria-
CC1582-100 Flood Consequences Assessment and Drainage Strategy

CC1582 Transportation Statement Report
CC1582-200 Drainage Strategy
CC1582-1002 P2 Vehicle Tracking Refuse Plan
Terraforma-
Geotechnical and geo-environmental site investigation report
TDA Arboricultural Method Statement June 2017 (updated November 2017)
Detailed Soft Landscape Proposals TDA.2027.05 Rev B
Arboricultural Impact Assessment (A)(2)
TDA 2027TS&ARhC03-14 Pre-Development Tree Survey and Assessment
TDA 2027TCPRhC03-14 Tree Constraints Plan
TDA.2027.06 Tree Pit Details: Soft Landscaping Area
Landscape Specification and Management Plan November 2017 received 4/12/17
Tim O`Hare-
16-5232-3-LHJ Soil Resource Survey Report
Just Mammals-
GEN10813-1 Highfields Bat Report
GEN10813-1 Highfields Phase 1 Ecological Report
GEN10813 Update Letter – Highfields
Wardell Armstrong AB.CA11282.LET-001 Preliminary Ground Level Roost Assessment for Bats
Wardell Armstrong Noise Assessment Report Job Number CA11311 Report Number 001

Reason: To avoid doubt and confusion as to the approved plans.

3. Prior to the commencement of any development works a scheme to investigate and monitor the site for the presence of gases being generated at the site or land adjoining thereto, including a plan of the area to be monitored, shall be submitted to the Local Planning Authority for its approval.

Following completion of the approved monitoring scheme, the proposed details of appropriate gas protection measures to ensure the safe and inoffensive dispersal or management of gases and to prevent lateral migration of gases into or from land surrounding the application site shall be submitted to and approved in writing to the Local Planning Authority. If no protection measures are required than no further actions will be required.

All required gas protection measures shall be installed and appropriately verified before occupation of any part of the development which has been permitted and the approved protection measures shall be retained and maintained until such time as the Local Planning Authority agrees in writing that the measures are no longer required.

Reason: To ensure that the safety of future occupiers is not prejudiced in accordance with policy EN13 of the Cardiff Local Development Plan.

4. Prior to the commencement of the development a detailed remediation scheme and verification plan to bring the site to a condition suitable for the intended use by removing any unacceptable risks to human health, controlled waters, buildings, other property and the natural and historical environment shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include all works to be undertaken,

proposed remediation objectives and remediation criteria, a timetable of works and site management procedures. The scheme shall ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

All work and submissions carried out for the purposes of this condition shall be conducted in accordance with DEFRA and the Environment Agency's 'Model procedures for the Management of Land Contamination, CLR 11' (September 2004) and the WLGA / WAG / EA guidance document ' Land Contamination: A guide for Developers' (July 2012),.

Reason: To ensure that any unacceptable risks from land contamination to the future users of the land , neighbouring land, controlled waters, property and ecological systems are minimised, and to ensure that the development can be carried out safely without unacceptable risks to neighbours and other offsite receptors in accordance with policy EN13 of the Cardiff Local Development Plan

5. The remediation scheme approved by condition 4 shall be fully undertaken in accordance with its terms prior to the occupation of any part of the development unless otherwise agreed in writing by the Local Planning Authority. The Local Planning Authority shall be given two weeks written notification of commencement of the remediation scheme works.

Within 6 months of the completion of the measures identified in the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out shall be submitted to and approved in writing by the Local Planning Authority.

All work and submissions carried out for the purposes of this condition shall be conducted in accordance with DEFRA and the Environment Agency's 'Model procedures for the Management of Land Contamination, CLR 11' (September 2004) and the WLGA / WAG / EA guidance document ' Land Contamination: A guide for Developers' (July 2012).

Reason : To ensure that any unacceptable risks from land contamination to the future users of the land , neighbouring land, controlled waters, property and ecological systems are minimised, and to ensure that the development can be carried out safely without unacceptable risks to neighbours and other offsite receptors in accordance with policy EN13 of the Cardiff Local Development Plan.

6. In the event that contamination is found at any time when carrying out the approved development that was not previously identified it shall be reported in writing within 2 days to the Local Planning Authority, all associated works shall stop, and no further development shall take place unless otherwise agreed in writing until a scheme to deal with the contamination found has been approved. An investigation and risk assessment shall be undertaken and where remediation is necessary a remediation scheme and verification plan must be prepared and submitted to and approved in writing by the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report shall be submitted to and approved in writing by the Local Planning Authority. The timescale for the

above actions shall be agreed with the Local Planning Authority within 2 weeks of the discovery of any unsuspected contamination.

Reason: To ensure that any unacceptable risks from land contamination to the future users of the land, neighbouring land, controlled waters, property and ecological systems are minimised, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy EN13 of the Cardiff Local Development Plan.

7. Any topsoil [natural or manufactured], or subsoil, to be imported shall be assessed for chemical or other potential contaminants in accordance with a scheme of investigation which shall be submitted to and approved in writing by the Local Planning Authority in advance of its importation. Only material approved by the Local Planning Authority shall be imported. All measures specified in the approved scheme shall be undertaken in accordance with the relevant Code of Practice and Guidance Notes.

Subject to approval of the above, sampling of the material received at the development site to verify that the imported soil is free from contamination shall be undertaken in accordance with a scheme and timescale to be agreed in writing by the Local Planning Authority.

Reason: To ensure that the safety of future occupiers is not prejudiced in accordance with policy EN13 of the Cardiff Local Development Plan.

8. Any aggregate (other than virgin quarry stone) or recycled aggregate material to be imported shall be assessed for chemical or other potential contaminants in accordance with a scheme of investigation which shall be submitted to and approved in writing by the Local Planning Authority in advance of its importation. Only material approved by the Local Planning Authority shall be imported. All measures specified in the approved scheme shall be undertaken in accordance with the relevant Code of Practice and Guidance Notes.

Subject to approval of the above, sampling of the material received at the development site to verify that the imported material is free from contamination shall be undertaken in accordance with a scheme and timescale to be agreed in writing by the Local Planning Authority.

Reason: To ensure that the safety of future occupiers is not prejudiced in accordance with policy EN13 of the Cardiff Local Development Plan.

9. Any site won material including soils, aggregates, recycled materials shall be assessed for chemical or other potential contaminants in accordance with a sampling scheme which shall be submitted to and approved in writing by the Local Planning Authority in advance of the reuse of site won materials. Only material which meets site specific target values approved by the Local Planning Authority shall be reused.

Reason: To ensure that the safety of future occupiers is not prejudiced in accordance with policy EN13 of the Cardiff Local Development Plan.

10. No building shall be occupied until the drainage system for the site as shown on drawing reference CC1582 200 Rev P2 has been completed in accordance with the approved details. Thereafter, Surface water flows from the development shall only communicate with the public surface water

sewer through an attenuation device that discharges at a rate not exceeding 5 l/s.

Reason: To prevent hydraulic overloading of the public sewerage system, to protect the health and safety of existing residents and ensure no pollution of or detriment to the environment.

11. The development shall be carried out in accordance with the Soil Resource Plan, planting plan, tree pit section, landscape specification, Arboricultural Method Statement and all planting, seeding, turf-laying and paving shown on the approved plans shall be carried out in the first planting and seeding season following the completion of the development.

Reason: In the interests of the visual amenities of the area.

12. Any newly planted trees, plants or hedgerows, which within a period of 5 years from the completion of the development die, are removed, become seriously damaged or diseased, or in the opinion of the Local Planning Authority otherwise defective, shall be replaced in the first available planting season and to the specification shown on approved plans and in supporting documents.

Reason: In the interests of the visual amenities of the area.

13. No development shall commence on the new substation until a planting scheme around the substation has been submitted to the Local Planning Authority for approval and shall then be implemented as approved within six months of the substation being erected.

Reason: In the interests of the visual amenities of the area.

14. Prior to any development commencing on site details of a scheme for providing vehicular access through the application site to the garden centre during the construction period shall be submitted to the Local Planning Authority for approval and shall then be implemented as approved.

Reason: To safeguard vehicular access to the garden centre.

15. The garden centre parking area identified on the approved plans shall be constructed prior to any other development commencing on the existing parking area to the garden centre.

Reason: To safeguard vehicular parking for the garden centre.

16. Prior to the development being brought into beneficial use a scheme to incorporate keep clear hatching for the turning area and access to the garden centre to the rear of dwellings 11 and 12 shall be submitted to and approved in writing by the Local Planning Authority and shall then be implemented as approved.

Reason: To safeguard vehicular parking for the garden centre.

17. Prior to the commencement any development a scheme (Construction Environmental Management Plan) to minimise dust emissions and minimise the impact on the highway arising from construction activities on site during the construction period shall be submitted in writing for approval by the Local Planning Authority. The scheme shall include (but not be limited to) details of site hoardings, site access and wheel washing facilities, a strategy for the delivery of plant and materials, construction staff parking, traffic management proposals and details of dust suppression measures and the

methods to monitor emissions of dust arising from the development. The demolition and construction phases shall be implemented in accordance with the approved scheme, with the approved dust suppression measures being maintained in a fully functional condition for the duration of the demolition and construction phases.

Reason: To safeguard the amenity of nearby businesses in the area and highway safety.

18. Prior to beneficial occupation details of the bicycle and bin stores shall be submitted to and approved in writing by the Local Planning Authority then constructed as approved prior to any apartment being brought into beneficial use and shall be so retained thereafter.
Reason: To ensure satisfactory bicycle and bin storage is provided and then retained for the use of future occupiers.
19. Prior to development commencing on site details of the means of blocking off the pedestrian access from Highmead shall be submitted to the Local Planning Authority for approval and shall then be implemented as approved.
Reason: Insufficient details have been submitted and in the interests of security.
20. No part of the development hereby permitted shall be commenced until a scheme of reinstatement works to Allensbank Road and Heathmead adjacent to the site has been submitted to and approved in writing by the Local Planning Authority. The scheme to include (but not be limited to) the resurfacing/reinstatement of the footway/carriageway as may be required as consequence of implementation of the development; to include the provision of a new road access off Allensbank Road, closure of the existing access to the garden centre, provision of standard height kerbing, new pedestrian access from Heathmead, closure of vehicle access to Heathmead. These works would include, but not be limited to, road surfacing, kerbs, edging, (tactile) paving, drainage, lighting, road marking & lining, signing and street furniture as required as a consequence of the scheme. No part of the development shall be occupied until the approved scheme has been implemented.
Reason: To amend and reinstate the carriageway and footway to provide an improved environment to facilitate safe commodious access to and from the proposed development.
21. A 1.8m high close boarded fence shall be erected along the length of the eastern boundary of the site from the southern edge of the garden centre parking area to the south eastern corner of the site prior to the occupation of any dwelling on this site.
Reason: In the interests of privacy and security.
22. Triple Glazing in all noise sensitive rooms on the front façade onto Allensbank Road shall be installed as detailed in Report Number 001, Job Number CA11311; 4-20-4-20-4 Non-openable prior to the beneficial occupation of these dwellings.
Reason: In the interests of the amenities of future occupants.
23. Zehnder Comfoair 160 ventilation units shall be installed as detailed in Report Number 001, Job Number CA11311 prior to the beneficial occupation of the dwellings..

Reason: In order to ensure adequate ventilation for future occupants

24. A noise assessment shall be carried out and submitted to the Local Planning Authority to ensure the noise emitted from fixed plant and equipment on the site achieves a rating noise level of background - 10dB at the nearest noise sensitive premises when measured and corrected in accordance with BS 4142: 2014 (or any British Standard amending or superseding that standard).

Reason: To ensure that the amenities of occupiers of other premises in the vicinity are protected.

25. No development shall commence on the houses or apartments until samples of the external materials have been submitted to the Local Planning Authority for approval and shall then be implemented as approved.

Reason: In the interests of the visual amenities of the area.

RECOMMENDATION 2 : The contamination assessments and the effects of unstable land are considered on the basis of the best information available to the Planning Authority and are not necessarily exhaustive. The Authority takes due diligence when assessing these impacts, however you are minded that the responsibility for

- (i) determining the extent and effects of such constraints;
- (ii) ensuring that any imported materials (including, topsoils, subsoils, aggregates and recycled or manufactured aggregates/ soils) are chemically suitable for the proposed end use. Under no circumstances should controlled waste be imported. It is an offence under Section 33 of the Environmental Protection Act 1990 to deposit controlled waste on a site which does not benefit from an appropriate waste management license. The following must not be imported to a development site;
 - Unprocessed / unsorted demolition wastes.
 - Any materials originating from a site confirmed as being contaminated or potentially contaminated by chemical or radioactive substances.
 - Japanese Knotweed stems, leaves and rhizome infested soils. In addition to section 33 above, it is also an offence under the Wildlife and Countryside Act 1981 to spread this invasive weed; and
- (iii) the safe development and secure occupancy of the site rests with the developer.

Proposals for areas of possible land instability should take due account of the physical and chemical constraints and may include action on land reclamation or other remedial action to enable beneficial use of unstable land.

The Local Planning Authority has determined the application on the basis of the information available to it, but this does not mean that the land can be considered free from contamination.

RECOMMENDATION 3 : To protect the amenities of occupiers of other premises in the vicinity attention is drawn to the provisions of Section 60 of the Control of Pollution Act 1974 in relation to the control of noise from demolition and construction activities. Further to this the applicant is advised that no noise audible outside the site boundary adjacent to the curtilage of residential property shall be created by construction activities in respect of the implementation of this consent outside the hours of 0800-1800 hours Mondays to Fridays and 0800 - 1300 hours

on Saturdays or at any time on Sunday or public holidays. The applicant is also advised to seek approval for any proposed piling operations.

RECOMMENDATION 4 : The new garden centre signs on the public highway will need to be applied for and approved by the Highway Authority as this would not be part of the planning permission.

RECOMMENDATION 5 : No highways work can be undertaken without written agreement from the Council's S278 / S38 teams.

RECOMMENDATION 6 : The developer shall have regard to the consultation responses received during the processing of this application.

RECOMMENDATION 7 : Prior to the commencement of development, the developer shall notify the Local Planning Authority of the commencement of development , and shall display a site notice and plan on, or near the site, in accordance with the requirements of Article 12 of the Town & Country Planning (Development Management Procedure)(Wales)(Amendment) Order 2016.

1. **DESCRIPTION OF DEVELOPMENT**

1.1 The applicant in the Design Statement describes the approach to the overall HPP approach as follows:

The aim of the HPP is to build around 1500 mixed tenure, energy efficient, sustainable and high quality homes across 40 Council owned sites within Cardiff. Sites vary in size from a few dwellings to almost 300. Overall, the programme will provide 40% affordable homes across the portfolio of sites with the remaining dwellings being placed on open market sale. The HPP will be delivered in three phases, within a period of 10 years and to the following key principles:

- A high level of energy efficiency in all homes and tenures that will assist with tackling fuel poverty through a fabric first approach.
- A tenure-blind, high quality of design and architectural standard.
- A high standard of urban design and place making.
- Sustainable communities by creating well connected and inclusive developments.
- High quality building standards and specifications for the affordable units.

1.2 The proposal is for a total of 42 dwellings comprising 12 x two bedroom houses for the open market and 20 x two bedroom apartments for the open market and 10 x affordable houses (5 x one bedroom and 5 x two bedrooms). The houses would have a floor area of 72.5 sq m and the apartments will range in size from 53 sq m (one bedroom) to 64 sq m for the affordable apartments and between 61sq m – 63 sq m for the open market apartments.

1.3 The houses would be built as two terraces of six on either side of a relocated bellmouth access road fronting Allensbank Road. The houses would be separated from Allensbank Road by a 0.9m high stone wall and front garden some 1.2m deep. To the rear of the houses there would be a private rear garden enclosed by a 1.8 m high brickwall and 1.8m high close boarded fence adjoining the car parking space. Car parking spaces would be accessed from an internal access road. Each house would accommodate a lounge opening to a kitchen/dining area and separate WC on the ground floor with two bedrooms a bathroom and en-suite above. The roofs would have Marley Ashmore old English dark red tiles with walls

of roughcast render. Windows would be triple glazed and the frames grey. The front entrance door would be edged by a double course of bricks. There would be a brick plinth of red stock brick up to the bottom of the ground floor windows. Each house would have a chimney.

- 1.4 The front of the proposed houses would be 23m from the houses opposite and separated by Allensbank Road. The proposed houses would be 8.4m high and each terrace would be 44m long, stepped to take account of the gradual fall across the site.
- 1.5 The apartments would be accommodated in a single building that is part two storey and part three storeys. The three storey element fronts the internal access road and most of the parking spaces serving the apartments. The front of the apartment building is set 36m back from Allensbank Road. The rear of the apartments is just over 11m from the boundary with dwellings in Heath Mead Road.
- 1.6 The ground and first floor apartments would be single aspect either facing west or east and the second storey units would have dual aspect. The first floor apartments would each have a small balcony (approximately 3.8 sq m). These balconies would be at their nearest 21m from the rear walls of the dwellings in Heath Mead Road and 24m from the rear walls of the proposed houses. The second floor element is set in 6.5 m from the second floor rear elevation. Each of the six apartments in the second floor will have a roof terrace of some 16 sq m set in 4m from the first floor rear elevation.
- 1.7 Each apartment would have a combined lounge/dining/kitchen and bathroom with 25 units having two bedrooms and 5 having one bedroom. The end apartments would contain secondary lounge windows overlooking the side parking areas (including the garden centre parking).
- 1.8 The apartments would have flat roofs on two levels. The rear wall of the apartments would be 7.5m high. The front wall of the apartments to the top of the roof would be 10.2m high. The apartment building would be 62m long and 14.8m wide. The walls would be finished in off white scraped render and incorporate timber cladding features around the entrances and balconies and to the walls of the upper storey.
- 1.9 Eleven trees are to be planted within the site, 7 of which would be planted along the eastern boundary of the site to supplement the 9 existing trees to be retained along this boundary (in total approximately one tree every 6m).
- 1.10 The affordable apartments would have a communal amenity area of 226 sq m. The ground floor open market apartments would each have rear gardens of just over 100 sq m
- 1.11 The Housing Officer has confirmed that:-

“We have specified 10 council flats and the allocations decision is that these will be designated for older people. There are no specific design requirements relating to this as the layout for older persons flats are the same for general needs. Please note the block is not a ‘Sheltered Housing’ scheme. When we decided to do a PassivHaus scheme we felt that this would work better as an older person block. I do not think this has to be stated in the planning application as it’s simply a Housing Management designation and we can change the designation of blocks at any time.”

- 1.12 One parking space is proposed for each dwelling (one of which is a disabled space), with 5 visitor parking spaces and 14 spaces (one of which is a disabled space) for the garden centre.
- 1.13 Two bike stores are to be provided, one for the affordable units (10 spaces) and one for the open market units (20 spaces).
- 1.14 A new central vehicular access is proposed onto Allensbank Road approximately 22m south of the existing access. The existing vehicular access from Heathmead is to be blocked off to vehicular use and replaced by a pedestrian access. The internal road would be primarily finished in block paving with some tarmac areas. The parking areas in front of the apartments and at the rear of the dwelling would in the main be differentiated by a different material or colour material. There are two turning heads at either end of the access road.
- 1.15 The houses, apartments, cycle and bin stores would have a combined footprint covering 1651 sq m (the former building on this site had a footprint of 1856 sq m).
- 1.16 The substation is to be relocated to the south eastern corner of the site.

2. **DESCRIPTION OF SITE**

- 2.1 The site lies within the Heath ward. The population of the ward taken at the 2011 census was 12,629.
- 2.2 The area is characterised by predominantly inter-war semi-detached housing with. There is extensive use of brickwork and render with tiled, hipped roofs. Pockets of more recent developments also exist within close proximity to the site, including the Heathmead estate which lies to the east and consists of a range of dwelling types.
- 2.3 To the northern boundary sits an existing Victorian villa which houses the Oaks Garden Centre. Parking for this is currently provided within the application. To the east lies Heathmead, and to the south and west lie predominantly two storey dwellings. The site can be currently accessed from Allensbank Road and Heathmead residential estate. The site is largely unused and comprises a grassed area, boundary planting, trees and shrub vegetation.
- 2.4. Currently, there are two vehicle access points, one from Allensbank Road and the other from Heathmead, albeit near its junction with Allensbank Road.

Pedestrians can access the site from a further two points, along a footpath that links to Heathmead and from The Oaks Garden Centre to the north.
- 2.5 The Heathmead bungalows lie close to the site's eastern boundary.
- 2.6 The boundary with Allensbank Road is marked by a stone wall of varying height which adds character to the site's edge and a material link to the garden centre building to the north.
- 2.7 The site is 0.69 ha in area and rectangular in shape. The site has a slight fall from north to south. There are a number of trees along the northern and eastern boundary of the site and with the Oaks garden centre to the north. There are 9 individual trees close to or adjoining the site frontage and a group of trees in the

south west corner of the site. Three of these frontage trees are of moderate value, five of the trees and the group of trees are of low value and one tree has been identified in the Tree Constraints Plan as requiring removal.

- 2.8 There are double yellow lines directly in front of the site on Allensbank Road. Most of the semi-detached dwellings on Allensbank Road facing the site have two off street parking spaces. There is no on street parking control on the western side of this part of Allensbank Road except for a small section of residents parking between the hours of 8.00am – 10pm opposite the entrance to Heathmead.
- 2.9 The dwellings in Highfield Road close to the application site each have at least one off street car parking space and a garage. There are no parking limitations on this section of road. At the entrances to Heathmead and Heathwood Grove there are signs saying the streets are covered by a parking permit control zone between 8.00 am – 10 pm.
- 2.10 During visits to the site cars were observed parked on street between driveways on the western side of Allensbank Road and Highfield Road near the site, filling all potential on street parking spaces.
- 2.11 There is an existing parking area for 20 spaces that serves the Oaks that gains vehicular access through the application site.
- 2.12 There is a bus stop 50 m north of the application site and another 125m south of the site both on the eastern side of Allensbank Road. There is a bus stop on the western side of Allensbank Road opposite the Oaks. The bus service along Allensbank Road is the 86. There is a regular bus service to and from the bus stops at Heath hospital to various parts of Cardiff, which is 500m distant from the application site. The bus stops at Roath Park Lake are 460m distant. The Heath railway station is approximately 700m north of the application site.
- 2.13 There is a corner shop on the western side of Allensbank Road 40m from the site next to a zebra crossing. The Heath Hospital is at its nearest point and in a straight line 250 m distant from the application site, Heath Park is approximately 100m distant, Highfields allotments is 350m distant, Roath Park Lake is 400 m distant and the site is 1Km from Allensbank Primary School.
- 2.14 Allensbank Road north of the A48 is characterised first by semi-detached houses then by two terraces of 14 dwellings each and then by semi-detached houses opposite the application site. Heathmead is set back from Allensbank Road and comprises a mixture of bungalows, two detached, semi-detached, terraced houses (up to six units in length) and apartment units. (The bungalows adjoining the site in Heathmead are Council properties). The character of the area changes as Allensbank Road becomes Heath Park Avenue, which comprises primarily semi-detached and detached houses. There is a three storey building at Highview on Highfield Road that is 7.5m from the side rear boundary of a dwelling in Heathmead, granted consent in 2002 (ref 02/01574/N). There is a part three storey development at Heath Park Court (the ground falls away to the north and this building is at a lower level than the dwellings in Highfield Road).
- 2.15 There are relatively few trees in the gardens of dwellings along Allensbank Road north of the A48. There are trees within the cemetery that front Allensbank Road, a group of trees that screen the rear of 4 dwellings that are in an elevated position at

the entrance to Heathmead and a group of trees to the side and rear of 225 King George V Drive East at the entrance to the Heath Hospital.

- 2.16 There is an electricity substation within the front of the site screened by planting on three sides.

3. **PLANNING HISTORY**

- 3.1 12/00213/DCO - Outline residential withdrawn.
- 3.2 This application was reported to the Planning Committee in April 2012 and Members were minded to approve subject to a Section 106 Agreement which was not concluded. The application was accompanied by three options; option 1 for 27 dwellings, the second for 14 units and the third for 17 units. Proposed condition 24 specified that the permission would not extend to more than 27 units as the implications of considering more had not been considered.
- 3.3 Option 1 specified 27 units, 16 four bedroom units and 11 three bedroom units (a total of 97 bedrooms). This option contained a mixture of houses and flats in a 2-3 storey block. The nearest part of the development to the boundary with the adjoining bungalows was approximately 5m distant. The maximum height was specified as 9m and maximum length for the terrace option as 80m. A proposal to limit the size of the development to two storeys was not accepted by Committee.
- 3.4 The Planning Officer's report identified option 1 as a preferred option but did not commit the Authority to approving a detailed proposal as per option 1.
- 3.5 The Planning Officer's report recognised that none of the options may be considered in keeping with the surrounding area but that provided appropriate detailing and materials were used the development would not be out of place and make more efficient use of the land.
- 3.6 12/2007/DCO - Demolition of Highfields Centre for the elderly and physically disabled approved 10/1/13.

4. **PLANNING POLICY**

- 4.1 It is considered that the following LDP policies are relevant to this application:-

KP1, KP5, KP7, KP8, KP14, KP16, H3, H6, EN8, EN13, T1, T5, T6, C1, C3, C5 and W2

SPG Planning for Health and Well Being
SPG Access, Circulation and Parking Standards
SPG Residential Design Guide
SPG Affordable Housing
SPG Biodiversity
SPG Community Facilities and Residential Development
SPG Planning Obligations
SPG Protection and Provision of Open Space
SPG Green Infrastructure Trees and Development
SPG Infill Sites

5. **INTERNAL CONSULTEE RESPONSES**

5.1 (a) Contaminated Land state:-

The following information has been submitted as part of the application:

Terra Firma (Wales) Ltd, March 2016; Geotechnical and Geo-environmental Report:Ref:13489.

The above report confirms that a contamination assessment has been undertaken based on a detailed desk study and on-site exploratory holes, sampling and testing.

The desk study identifies areas of potential infilled ground on site (former filter beds) and immediately offsite (former reservoir). The onsite investigation identifies no risk of ground gases from the made ground within the area of the former filter beds.

There has been no assessment of the risks from potential ground gas migration from made ground within the area of the former reservoir (via either off site exploratory investigation or on site/ off site ground gas monitoring). Further assessment is therefore required.

The above report identifies localised contamination from lead and arsenic has been identified and general guidance/recommendations have been provided in relation to this, and possible removal of the material. A clear remediation strategy will need to be confirmed with this regard.

Should there be any importation of soils to develop the garden/landscaped areas of the development, or any site won recycled material, or materials imported as part of the construction of the development, then it must be demonstrated that they are suitable for the end use. This is to prevent the introduction or recycling of materials containing chemical or other potential contaminants which may give rise to potential risks to human health and the environment for the proposed end use.

Shared Regulatory Services requests the inclusion of the following conditions and informative statements in accordance with CIEH best practice and to ensure that the safety of future occupiers is not prejudiced in accordance with policy EN13 of the Cardiff Local Development Plan.

(b) Contaminated Land has agreed that one of their originally suggested conditions can be removed but all their other conditions in respect of remediation are to be retained.

5.2 The Tree Officer has no adverse observations to the amended landscaping details.

5.3 Neighbourhood Services Officer has raised no objection on grounds of noise and has suggested a number of conditions incorporated into the Recommendation. A condition in respect of the gardens being screened by the dwellings and 1.8m high screen walls/close boarded fence is also proposed. This follows the submission of a noise report.

5.4 Parks Officer states:

Design Comments

Overall I welcome the design, which makes good use of the site without impacting hugely on adjacent dwellings. The boundary trees have largely been retained with additional tree planting provided to increase the screening. The communal garden means that the apartments are set back from the properties behind. I welcome retention of the stone boundary wall at the front of the site, which has always been a key feature of the site and will provide a good buffer to Allensbank Road which is extremely busy.

The management arrangements for the site, set out on the management plan, look to be workable. I agree with the proposal to include the entrance planting as part of the management company responsibilities rather than forming part of the adopted highway. This might need a S142 licence under the highway adoption process.

Open Space Provision

These comments relate to the current LDP (C5 Provision for Open Space, Outdoor Recreation, Children's Play and Sport; KP16 Green Infrastructure), and the 2017 Planning Obligations Supplementary Planning Guidance (SPG), supported by policies set out in the 2008 SPG for Open Space which set the Council's approach to open space provision.

The Council's LDP requires provision of a satisfactory level and standard of open space on all new housing/student developments, or an off-site contribution towards existing open space for smaller scale developments where new on-site provision is not applicable.

Based on the information provided on the number and type of units, I have calculated the additional population generated by the development to be 73.1. This generates an open space requirement of 0.1776 ha of on-site open space based on the criteria set for Housing accommodation, or an off-site contribution of £75,845. I enclose a copy of the calculation

As no public open space is being provided on-site, the Council considers it appropriate that an off-site contribution is made towards the provision of new open space, or the design, improvement and/or maintenance of existing open space in the locality, given that demand for usage of the existing open spaces would increase in the locality as a result of the development

The use of S106 contribution from this development will need to satisfy CIL and the current distance requirements set out in the 2017 Planning Obligations SPG – play areas 600m (not applicable to student and sheltered accommodation), informal recreation 1000m, and formal recreation 1500mm, measured from edge of the site.

In the event that the Council is minded to approve the application, I assume it will be necessary for the applicant and the Council to enter into a Section 106 Agreement to secure payment of the contribution.

Consultation would take place with Ward Members to agree use of the contribution, and this will be confirmed at S106 stage. The closest areas of recreational open space are Heath Park, Cathays Cemetery and Llwynfedw Gardens

5.5 (a) The Waste Management Officer states

Houses

Each property will require the following for recycling and waste collections:

- 1 x 140 litre bin for general waste
- 1 x 240 litre bin for garden waste
- 1 x 25 litre kerbside caddy for food waste
- Green bags for mixed recycling (equivalent to 140 litres)

Affordable Housing Apartments (Plots 33-42)

These apartments will require the following for waste and recycling collections for 10 apartments:

- 1 x 1100L bin for general waste
- 1 x 1100L bin for recycling
- 1 x 240 litre bin for food waste
- 1 x 240 litre bin for garden waste

Open Market Housing Apartments (Plots 13-32)

These apartments will require the following for waste and recycling collections for 20 apartments

- 2 x 1100L bin for general waste
- 2 x 1100L bin for recycling
- 1 x 240 litre bin for food waste
- 1 x 240 litre bin for garden waste

We have noted the proposed position of the communal bin stores, these are acceptable.

Communal bin stores should have double doors that open outward. Surfaces should be smooth and impervious to permit cleaning and the floor must be laid to create suitable drainage. Adequate artificial lighting must be provided and good natural ventilation if completely enclosed

The developer is advised; as bulk containers are specified for this development, access paths to the kerbside for collection should be at least 1.5 metres wide, clear of obstruction, of a smooth surface with no steps. Dropped kerbs should also be provided to ensure safe handling of bulk bins to the collection vehicle.

Waste Management will not carry keys for bin storage areas; so waste must either be presented at the entrance to the development for collection, or the access gates to the bin store must be left open. It may be possible for the crews to use access codes, provided they are not changed too often.

Bulk containers must be provided by the developer/other appropriate agent, to the Councils' specification (steel containers are required where capacity exceeds 240 litres) as determined by S46 of the Environment Protection Act 1990 and can be purchased directly from the Council. Please contact the Waste Management's commercial department for further information on 02920 717500.

Please note, the collection method utilised by Cardiff Council will likely change in the future which could result in different/more receptacles being required for the storage of waste between collections. In light of this, the bin store areas should allow a degree of flexibility in order to be able to adapt to potential collection changes.

General comments

Please advise the owner/applicant that since 27th July 2015, the developers of all new residential units are required to purchase the bin provision required for each unit. The bins have to meet the Council's specifications and can be purchased directly by contacting the Waste Management's commercial team on 029 20717500.

The kitchens should be designed to allow the separation of waste into three waste streams; general, recycling and food waste, in order to encourage the correct disposal of waste.

Please refer the agent/architect to the Waste Collection and Storage Facilities Supplementary Planning Guidance for further relevant information.

(b) The Waste Management Officer further responded that she was happy with the revised access arrangements as the refuse vehicles will be able to reverse on site as it's only a short distance

5.6 The Air Quality Team state:

I have examined the submitted documentation in support of planning application 17/02464/MJR from an air quality perspective. Examining the proposed plans I am satisfied that little risk is placed upon future residents and nearby residents in terms of traffic derived emissions. As concluded within the submitted transport report (CC1582/REP02/E); It is anticipated that the development will generate up to 11 peak hour vehicle movements. This will result in one additional vehicle movement every 5 and a half minutes on average during peak times. This level of traffic will be imperceptible in the context of existing traffic volume and the existing day to day fluctuations in traffic flow and will have no material impact on the operation of the highway.

Reviewing the application from a dust emissions perspective, due to the close proximity of residential dwellings to the proposed development it is considered best practise to adopt the principles stipulated in IAQM "Guidance on the assessment of dust from demolition and construction." The guidance provides a risk based approach based on the potential dust emission magnitude of the site (small, medium or large) and the sensitivity of the area to dust effects. The importance of professional judgment is noted throughout the guidance. The guidance recommends that once the risk class of the site has identified, the appropriate level of mitigation measures are implemented to ensure that the construction activities have no significant impacts. In accordance with the guidance, Chapter 6, Step 1, Box 1 highlights certain screening criteria which needs to be considered and if a development qualifies for an assessment. The document states "An assessment will normally be required where there is: a 'human receptor' within: - 350 m of the boundary of the site; or- 50 m of the route(s) used by construction vehicles on the public highway, up to 500 m from the site entrance(s)." It is apparent that there are residential dwellings located in close proximity to the proposed site, therefore satisfying the 'human receptor' criteria stipulated in the cited guidance and the need for a detailed dust assessment to be produced.

Condition: Dust Control

Unless otherwise agreed with the LPA, the applicant is required to undertake a detailed dust assessment which shall quantify the magnitude of risk to

surrounding/ nearby sensitive receptors, this being the various residential properties located within 350m to the site boundary, during the construction phase of the development. Prior to the commencement of development a scheme (Construction Environmental Management Plan) to minimise dust emissions arising from construction activities on the site shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of dust suppression measures and the methods to monitor emissions of dust arising from the development. The construction phase shall be implemented in accordance with the approved scheme, with the approved dust suppression measures being maintained in a fully functional condition for the duration of the construction phase. Reason: To assess air quality and agree any mitigation measures that may be required to safeguard the amenity of nearby residents in the area.

5.7 The Affordable Housing Officer states

This scheme forms part of the Council's Cardiff Living Project (*formerly the Housing Partnership Programme (HPP), which is a partnership between the Housing Development Team and Wates Living Homes. The aim of the project is to build around 1,500 mixed tenure, energy efficient, sustainable and high quality homes across c. 40 Council owned sites within Cardiff. Overall, the programme will provide 40% affordable homes across the portfolio of sites with the remaining dwellings being placed on open market sale by Wates Living Homes.

In terms of the affordable housing, the proposal is for 10 of homes to be affordable, to be owned by Cardiff Council for social rented accommodation and/or low cost home ownership (LCHO).

This proposal exceeds the planning policy requirements of 20% on brownfield sites, and we are fully supportive of this affordable housing scheme.

5.8 Regeneration state:-

Request for Community Facilities

The Cardiff Planning Obligations SPG 2017 (Section 8 – Community Facilities) states that 'Growth in population arising from new development generates demand for and increases pressure on community facilities. To meet the needs of future residents, it may be necessary to meet this additional demand through:

The provision of new facilities,
The extension to, or upgrading of existing facilities'.

If no onsite provision is proposed, a financial contribution is sought on residential developments containing 25 or more new dwellings where it has been identified that investment in community facilities will be required to meet the needs of the new population.

The formula in the SPG is based on the number of bedrooms and associated occupancy figures per dwelling, and is calculated as follows:

No. of bedrooms	Number of Dwellings	Contribution per dwelling	Totals
1	5	£720.51	£3,602.55
2	37	£997.63	£36,912.31
3		£1,385.60	
4		£1,718.14	
TOTALS:			£40,514.86

In summary, the following contribution is sought from the developer:
£40,514.86

CIL Regulation 122(2) provides:

A planning obligation may only constitute a reason for granting planning permission for the development if the obligation is—

- (a) necessary to make the development acceptable in planning terms;
- (b) directly related to the development; and
- (c) fairly and reasonably related in scale and kind to the development.

Comments in respect of the request for contributions towards community facilities are as follows:

(a) Necessary to make the development acceptable in planning terms:

The Cardiff Planning Obligations SPG was formally adopted by Council on 26th January 2017. The SPG was adopted to help to ensure that developments contribute toward the provision of necessary infrastructure and measures required to mitigate their impact. Policy KP6 of the Cardiff Local Development Plan (adopted January 2016) supports the provision of community facilities as part of new residential developments.

It is also in accord with Planning Policy Wales which supports the negotiation of planning obligations and states “Contributions from developers may be used to offset negative consequences of development, to help meet local needs, or to secure benefits which will make development more sustainable”. A development proposing a significant increase in population, such as this, would create pressures on existing local facilities that need to be offset via onsite provision or a financial contribution. It would be unacceptable to grant planning consent in the absence of such provision.

(b) Directly related to the development

Several community facilities are located within proximity to the site and are likely to experience an added pressure as a result of the new population. It is envisaged that a forthcoming community facilities contribution would be directed towards these facilities in the vicinity of the development.

(c) Fairly and reasonably related in scale and kind to the proposed development

Contributions towards community facilities are derived using a formula-based calculation which takes into account, amongst other things, the size of the residential development and the projected increase in population.

- 5.9 (a) The Transportation Officer says that all comments from previous layout appear to be covered. However should the “turning head / access” to garden centre incorporate keep clear hatching to protect access.

Junction access would be better as a bell-mouth rather than vehicle crossover given number of properties and garden centre traffic.

The new garden centre signs on the public highway will need to be applied for and approved in the appropriate manner as this would not be part of the planning permission.

(b) Cardiff Council has produced a Road Safety Audit which identified six issues. This identifies a requirement for a cross over at the access, which is now proposed; reinstating the access and addressing an existing adjoining projecting drain cover (which would be addressed by condition 20); and highway drainage, internal street lighting and shared surface signage can be covered later under the Highway agreement.

(c) Refuse Vehicle tracking information has been submitted by the applicant to the Highway Authority. The Transportation Officer has confirmed that refuse vehicles can manoeuvre within the proposed development.

5.10 The Education Officer states:-

I've checked the numbers, the total yield from the development would be below the threshold at which we would make a claim.

6. **EXTERNAL CONSULTEE RESPONSES**

6.1 South Wales Police state:

As stated in the Design and Access Statement a South Wales Police Designing out Crime officer has conferred with the applicant at an earlier date to discuss matters in respect of community safety and crime prevention.

The only observation I would like to add is that all communal entrances to apartments should have an entry phone / video link to each apartment with an electronic lock release door entry access control.

Reason to prevent unauthorised access and reduce the risk of burglary and anti social behaviour

South Wales Police operates the Secure by Design (SBD) initiative which is a National Police Chiefs Council and Home Office scheme which promotes the inclusion of crime prevention measures into developments. A safe and secure environment is the prime objective of the Secured by Design initiative. To achieve this result, equal weight should be given to both environmental design and physical security.

6.2 The Fire Safety Officer states inter alia that:-

Access for Fire Appliances

Typical vehicle access route requirements:

Appliance Type	Min Width Road	Min Width Gate	Min Turning Circle between Kerb
Pump	3.7m	3.1m	16.8m
Aerial Appliance	3.7m	3.1m	26.9m

Min Turning between Wall	Min Height Clearance	Min Capacity Tonnes
19.2	3.7m	12.5
29.0	4.0m	23

Pedestrian Priority

Pedestrian schemes must take into account the need for permanent and unobstructed access for firefighting appliances. The siting of ornamental structures such as flower beds, must take account, not only of the access requirements of the fire appliances but the need to be able to site them in strategic positions; in particular, account must be taken of the working space requirements for aerial appliances. Consultation must take place with the Fire Authority during the earliest planning stages of any development to ensure adequate access for fire appliances, their siting and use.

Water Supplies for Firefighting

The existing output of the statutory water supply network may need to be upgraded in certain parts of the local plan area to cater for firefighting needs of new developments. It is recommended that this provision be a condition of planning consent.

Access to Open Water Supplies

Consultation must take place with the Fire Authority during the earliest planning stages of any development to ensure access for fire pumping appliances is satisfactory

Housing

Minimum main size 100mm. Housing developments with units of detached or semi-detached houses of not more than two floors should have a water supply capable of delivering a minimum of eight litres per second through any hydrant on the development.

Housing developments with units of more than two floors should have a water supply capable of delivering a minimum of 20 to 35 litres per second through any hydrant on the development.

The distance between fire hydrants should not exceed the following:

Residential areas - 200 metres.

Developers should hold joint discussion with Dwr Cymru - Welsh Water or the National Rivers Authority and the Fire Authority to ensure that adequate water supplies are available in case of fire. the Fire Authority reserve the right to ask for static water supplies for firefighting on site as a condition of planning consent, if the supply infrastructure is inadequate for any given risk.

6.3 Welsh Water state:

We would request that if you are minded to grant Planning Consent for the above development that the Conditions and Advisory Notes provided below are included within the consent to ensure no detriment to existing residents or the environment and to Dwr Cymru Welsh Water's assets.

We have reviewed the information submitted as part of this application with particular focus on the Flood Consequence Assessment and Drainage Strategy reference CC1582/REP01/B and drawing number CC1582 200 Rev P2.

We note the details contained within the Drainage Strategy with regard to surface water and we understand that alternative options to utilise sustainable features have been discounted. The Terra Firma Geotechnical and Geo-environmental Site Investigation Report dated March 2016 outlines the results from the percolation tests and concluded that the infiltration rates recorded were not suitable. Whilst we acknowledge the investigations the report does not include the data to confirm the findings and therefore cannot be verified.

Notwithstanding the above we acknowledge that the proposed development is proposed on a brownfield site with the assumption that the surface water from the former building drained to the public sewer network. The proposed strategy provides attenuation to restrict the discharge rate and therefore based on the betterment provided we can agree to the proposal.

The Drainage Strategy refers to the existing on site drainage and further investigations required to determine its status. Whilst our records do not show any public sewers, the applicant is advised to establish and confirm the existing drainage is private before development commences. Should it be found that the drainage serves any other properties off site then the applicant should contact us direct to discuss.

Therefore, if you are minded to grant planning permission we request that the following conditions and advisory notes are included within any subsequent consent.

No building shall be occupied until the drainage system for the site as shown on drawing reference CC1582 200 Rev P2 has been completed in accordance with the approved details. Thereafter, Surface water flows from the development shall only communicate with the public surface water sewer through an attenuation device that discharges at a rate not exceeding 5 l/s.

Reason: To prevent hydraulic overloading of the public sewerage system, to protect the health and safety of existing residents and ensure no pollution of or detriment to the environment

The applicant may need to apply to Dwr Cymru / Welsh Water for any connection to the public sewer under S106 of the Water industry Act 1991. If the connection to the public sewer network is either via a lateral drain (i.e. a drain which extends beyond the connecting property boundary) or via a new sewer (i.e. serves more than one property), it is now a mandatory requirement to first enter into a Section 104 Adoption Agreement (Water Industry Act 1991). The design of the sewers and lateral drains must also conform to the Welsh Ministers Standards for Gravity Foul Sewers and Lateral Drains, and conform with the publication "Sewers for Adoption"- 7th Edition. Further information can be obtained via the Developer Services pages of www.dwrcymru.com

The applicant is also advised that some public sewers and lateral drains may not be recorded on our maps of public sewers because they were originally privately owned and were transferred into public ownership by nature of the Water Industry (Schemes for Adoption of Private Sewers) Regulations 2011. Under the Water Industry Act 1991 Dwr Cymru Welsh Water has rights of access to its apparatus at all times.

A water supply can be made available to serve this proposed development. The developer may be required to contribute, under Sections 40 - 41 of the Water Industry Act 1991, towards the provision of new off-site and/or on-site watermains and associated infrastructure. The level of contribution can be calculated upon receipt of detailed site layout plans which should be sent to the address above.

The proposed development is crossed by a 20" trunk watermain and 4" distribution watermain, the approximate position being shown on the attached plan. Dwr Cymru Welsh Water as Statutory Undertaker has statutory powers to access our apparatus at all times. I enclose our Conditions for Development near Watermain(s). It may be possible for this watermain to be diverted under Section 185 of the Water Industry Act 1991, the cost of which will be re-charged to the developer. The developer must consult Dwr Cymru Welsh Water before any development commences on site.

6.4 NRW raises no objection in respect of bats, land contamination and foul water disposal.

6.5 GGAT states:

Information in the Historic Environment Record, curated by this Trust, indicates that there are no recorded archaeological sites within the area of the proposed application. We note that the development area is situated approximately 200m to the north of Grade II* Cathays Cemetery Registered Historic Parks and Garden (Cadw Ref.: PGW (Gm) 19) and approximately 300m to the west of Roath Park Registered Historic Park and Garden (Cadw Ref.: PGW (Gm) 24).

The First Edition Historic Ordnance Survey map (1880) shows the proposed development area as an open field. The Second to Fourth Edition Historic Ordnance Survey maps (1901 to 1941) shows the area was later developed with a Sand Yard and associated tanks and structures present in the north of the area, along with Heath Filter Beds and Service Reservoirs structures, in the southern area, associated with the Cardiff Corporation Water Works. We note from the supporting documents that Highfields Centre appears on modern Ordnance Survey mapping dating from 1985 to 2015 and has since been demolished. Therefore, the area has previously been subjected to intensive construction and redevelopment and it is unlikely that any significant archaeological remains will be encountered during ground works associated with the development.

As a result, there is unlikely to be an archaeological restraint to this proposed development and consequently, as the archaeological advisors to your Members, we have no objections to the positive determination of this application. The record is not definitive, however, and features may be disturbed during the course of the work. In this event, please contact this division of the Trust.

6.6 The Design Commission for Wales has not submitted comments on this application but at pre-application stage on the scheme in mid- June 2016 stated inter alia:-

This development is part of a housing partnership scheme between Cardiff Council and Wates Homes with a £ 33m investment from the Council. There are over 40

sites across the city that are intended to deliver a total of 1500 homes over ten years in three phases .

This is the third of the housing partnership scheme sites to be reviewed by the Design Commission for Wales but the first time that this site has been reviewed. The following points were discussed in the review and should be given further consideration as proposals for the site progress.

Site layout

The process that led to the arrangement of the houses to the front of the site and three storey apartment block to the rear was discussed and the design team explained the rationale behind this decision - making process should be reflected in the Design and Access Statement that accompanies the application.

Access, circulation and way-finding

The requirement to retain access for visitors to the Garden Nursery car park and delivery point complicates the access and movement arrangements for the site. It is understood that separate access for this use has been explored but found not to work, therefore a suitable arrangement for directional signage, management and manoeuvring will need to be established.

It may be best to direct garden center visitors into the site from the south rather than through the western access point as there is more scope to sensitively locate signage.

The need for the second vehicle access point to the west of the site was questioned and should be considered in more detail. The extra space gained could create opportunities for improvements to the layout and quality of external spaces.

Bin storage

A suitable storage area for bins for the houses needs to be established taking account of the number of bins that people will have. The most suitable location may be to the rear of properties where a space can be built in adjacent to the parking area enabling all bin collections be undertaken along the central access road.

Bin stores for the apartment blocks need to be placed in a convenient location in order for them to be successful. The bin store to the north is somewhat isolated from the apartments and therefore may not be used properly.

Landscape strategy

There needs to be a clear and logical demarcation between the areas that are to be managed by property owners/residents and the management company. Diagrams to explain this arrangement would be helpful as it is currently unclear on the landscape plan. Measures, such as writing requirements into deeds, may need to be put in place to prevent the loss of features that are integral to the landscape strategy for the site. For example, the removal of boundary walls or planting along the rear edge of properties would have a significant impact on the character of the home zone.

The approach to hard and soft landscape design needs to balance the requirements for adoption of the highway with the desire to create an attractive and welcoming home zone setting for the dwellings. Currently the layout allows very little space for trees and soft landscaping within the street. Some minor changes to

the arrangement of the space around the apartment block could help to create more meaningful and usable spaces potentially including some play space and more room for residents to comfortably sit out in front of their properties.

When considering how the space within the site will be arranged, the potential for retaining existing trees should be established and a realistic plan prepared of what will and won't be retained.

The spaces to the front of the houses are small which may make planting unrealistic.

We would expect this boundary to be gated and a simple, but well executed, hard paving design may be most appropriate.

Orientation and overlooking

The corner properties need to provide overlooking of the route into the site and surveillance of the public realm particularly to the north of the apartment block. If there is not a door on this north elevation there should be windows from active rooms to help provide surveillance

Scale, mass and elevations

The row of terraced houses steps to accommodate a level change which also helps to break up the mass of the block to better reflect the semi-detached nature of the area. Options for managing the mass of the apartment block should also be explored. This may not be through a step in the roof line but through variation in the elevation. A central entrance with secondary entrances either side help to balance and break up the elevation. A three entrance strategy could also reduce the extent of internal shared corridors, leading to greater efficiency in the internal planning. These decisions would need to be balanced against the Passivhaus design requirements.

Further design development of the balconies is required. Using a 3D model to track the sun-path across the elevation will help to determine the depth, height and permeability of the walls that form the sides of the balconies on both the front and back of the block. The proposed solid form may prevent sunlight from penetrating the space and reduce daylight entering the properties. To the rear, the proposed form creates a frame that will focus the view onto the properties and back gardens of the houses to the rear of the site. An alternative arrangement could help to redirect or screen this view whilst capturing beneficial sunlight.

Visually the top floor of the apartment block seems divorced from the rest of the block. This is partly due to the windows, which look out from the circulation corridor, not responding to the fenestration arrangement below and also the change in material but no change to the profile of the building, resulting in a flat elevation. Pushing the top floor back could help to improve the roofscape.

Consideration should be given to the arrangement of the top floor as the corridor access doesn't seem to be the most efficient or effective access arrangement. Accessing these apartments from the three cores would be a better solution although management issues regarding the north core that would be shared between the affordable and private dwellings may need to be resolved.

Passivhaus design

Execution of the Passivhaus design is crucial and, through post-occupancy analysis, should be carried out to help the partnership to continue learning and improving for future schemes. Care should be taken with the internal arrangement of the houses if it is likely that residents will predominantly use their back door. A hallway space has been provided at the front but not at the back and this could have an impact on performance.

Next steps

The proposed development establishes a good use of the site and we are very supportive of this approach and in particular the piloting of Passivhaus for the housing partnership.

7. **REPRESENTATIONS**

- 7.1 Local Members have been notified.
- 7.2 The application has been advertised on site and in the press
- 7.3 Adjoining occupiers have been notified. Letters primarily of objections and some of concern have been received from Highfield Road (5), Allensbank Road (8), King George V Avenue (8), Heathwood Road (4), Heath Way (2), Heathmead (22), Heathwood Grove (5), St Edwyn Garden (1), St Benedict Crescent (2), Heath Park Avenue (15) St Gowan Ave (1), Maes y Coed Road (1) and unspecified locations as no address included in email (22).
- 7.4 Councillor Hinchey objects for the following reasons:-
 - 1) The density of the 42 units, comprising of 12 houses and 30 flats, is far too high. It is more than DOUBLE THE DENSITY of surrounding residential properties and nearly THREE TIMES the density of other similar Cardiff Living projects. For example Walker House, Llanishen has 16 units on a similar sized site.
 - 2) The 3 storey single block of 30 flats will have a severe and detrimental impact on the lives of the elderly, disabled and vulnerable Council tenants living in the single storey bungalows close by.
 - 3) The buildings are not in keeping or sympathetic to the characteristics of the surrounding residential area. All are one and two storey residential dwellings.
 - 4) The Passivhaus, low energy design will not have the correct effect and is completely out of orientation. The buildings are designed and planned in the wrong direction to benefit from sunlight trajectory. Instead of maximising sunlight from a south/north facing line the buildings run east/west.
 - 5) Traffic will be significantly increased in an area that has already been identified by the Council as a high accident zone.
 - 6) Existing UHW on street car parking problems for residents will be made much worse by the density of this site, with a total of only 46 car parking space for the 42 units and visitors, most of which are two bedroom units. Only 10 flats will be Council owned and affordable. What elderly resident would want to live on the 3rd floor with no lifts, no shops and very limited bus service? An estimated 100 cars will be generated by this majority open market development so close to the largest university hospital in Wales. It is anticipated that most will therefore be bought and rented out, making no contribution to Passivhaus Living or of benefit to the local community
 - 7) No Section 106 contributions are being suggested that could assist with implementing to Resident & Restricted parking, which is strongly supported by

residents who cannot park or have visitors, builders, deliveries or family or carers park near their properties anytime during the day Mon-Fri.

8) To make Passivhaus Living (green low energy/carbon style living) matters worse, there is a very limited bus service along Allensbank Rd/ Heath Park Avenue, so most will be car dependant purchasers. The existing service is hourly most days and ends early evening. The consultation document has many errors, for example, it includes a school bus service as a public transport option and nearby shops are half a mile away in Whitchurch Rd. In fact the nearest regular bus service is on Lake Road West, which is hundreds of metres away and up a very steep incline (Highfield Rd).

9) The Pre Consultation was not carried out to a good or acceptable standard, using the minimum 28 day period and held during the summer holiday period. Only the minimum number of directly adjacent residents were contacted, despite the impact on those in nearby properties who were unaware of the proposals. Some residents had multiple letters, some had none. Elderly and disabled residents were expected to read the single notice displayed on the entrance gate and visit Wilcox House in Cardiff Bay to view plans. Most do not have access to the internet and some do not have the physical skills or equipment to view online.

10) Despite the above, more than 130 written letters of objection from local residents have been submitted during the pre-consultation exercise, showing that residents feel strongly against this design and density being proposed

11) The impact on the Vision 21 Oaks Garden Centre, which has served as an adult learning centre for up to 35 students per week and for more than 20 years, will be severe and could close from being enclosed and "hidden away" from public view by this over development.

12) All elected Members, except one, have objected to this application. This includes both AM and MP for Cardiff North, the Heath Residents Association and two of the three local Councillors. Residents and elected Members are not against housing on this site but do feel 42 units and the single 3 storey block is completely out of character with all existing neighbouring residential buildings. The only other 3 storey building nearby is Heath Park Court, where the first floor is below natural ground level, having been built on an old railway siding. This building appears to be two storey from most directions of other residential properties.

In summary, I strongly object to these proposals on the grounds of poor design, high density, increased traffic and lack of public transport links and local amenities and the severe impact this proposal will have on our elderly, disabled and vulnerable residents living and working next to the proposed development.

7.5 Councillor Hudson has confirmed that she will be opposing this application.

7.6 Anna McMorrin MP has submitted a letter of objection stating that:-

I write as Member of Parliament for Cardiff North regarding my objections to the planning application 17 /02464/MJR Highfields housing development by Wates and Cardiff Living. I wish to register my objections to the development, which align with the stance I have taken during meetings with both Cardiff Council and the developers, Wates.

I would therefore like to state my clear objection for the development of Highfields site on Allensbank Road, Heath on the following grounds.

I am concerned about the density levels of the 42 units for this site, which is very high and well exceeds the density levels and scale of previous Cardiff Living/

Housing Partnership proposals. It is my understanding that this development, comprising of 12 houses and 30 flats, is three times the density of similar Cardiff Living projects and is double the density of the residential properties in the surrounding areas of the site.

Many of my constituents have raised concerns regarding the design of the buildings, especially the scale of the 3-storey single block of 30 flats. Inevitably, due to the design of the development, this will have a dramatic detrimental visual effect in the area, which is not in keeping with the character of the surrounding area. I am very concerned that this design has not considered the current housing type in the area nor the elderly, disabled and vulnerable Council tenants living in the surrounding single storey bungalows.

The demographic of the surrounding areas should also be given due consideration. Many of my constituents in Heathmead are elderly and may have mobility issues. Throughout this process, their anxiety regarding this development has been evident and we must keep in mind the detrimental impact this will have on those whose bungalows overlook the site, with a significant loss of privacy and daylight.

I very much welcome Cardiff Council's innovative approach taken to introduce an energy efficient Passivhaus development. However, when I raised the possibility with Wates of swapping the buildings around on the site or scaling down the development to remain in keeping with the surrounding area, the answer was unfortunately not a positive one. Wates advised that if they accepted a scaling down of the development, then they would be forced to remove the affordability element of the housing and would not be able to produce a Passivhaus Living development. This was very disappointing to hear as it is essential to find the balance between high energy efficient housing and minimising the effect on the local residents.

Many of my constituents have raised concerns regarding the levels of traffic which will significantly increase as a result of the development. It is my understanding that this is in an area that has already been identified by Cardiff Council as a high accident zone and so I am very concerned that the development will exacerbate this. Again, the demographics of the surrounding area need to be given due consideration, as many elderly constituents use mobility scooters and I am concerned about the risks posed to them by the increased traffic.

There have been longstanding issues with regards to the existing University Hospital of Wales on street car parking for residents, which many of my constituents fear will be exacerbated by the density of the site. There is a total of 46 car parking spaces for the 42 units and given that most of the units are two bedroom properties, there may be more than one vehicle per property.

My constituents have raised concerns regarding the overspill of parking on nearby roads such as Highfield Road, Heath Park Avenue and Heath Park Crescent and note that restrictions on parking are incredibly difficult to police. It is disappointing to hear that no Section 106 contributions have been earmarked to assist with the implementation of Resident and Restricted parking, which is strongly supported by residents who find it very difficult to park.

I am concerned about the lack of adequate public transport links within easy walking distance of the site. The current bus service is very poor, with irregular bus

times which seem to be worse in the late evenings and on weekends. I understand that the nearest regular bus service is on Lake Road West, which is hundreds of meters away from the development. The very limited bus service will inevitably exacerbate the parking issue at hand, as most purchasers of the units would be car dependent.

Some of my constituents who regularly volunteer in the Oaks Garden Centre have raised concerns about the development's impact on this, which has served as a vital adult learning centre for more than 20 years. The development could cause the Centre to be hidden away from public view which could negatively impact the charitable organisation which makes such a difference to members of the community.

It is my understanding that Wates was chosen by the Council for this development due to their good credentials for consulting people affected by their developments. However, throughout this process, the lack of input my constituents tell me they feel they have had with Wates is also evident in the contents of their objections. The pre-consultation was not carried out to an acceptable standard, only allocating the minimum 28-day period for consultation during the summer holidays. This is an incredibly busy time for most families and was not an ideal time to carry out a meaningful consultation.

I understand that some residents didn't even receive a letter with information on the proposal and with most of the residents in surrounding areas not having access to the internet, this presented a clear barrier for people to view the proposals or submit their comments in a timely manner.

To conclude, I strongly object to these proposals on the basis of the development's high density, poor design which is not in keeping with the area, increased traffic and parking issues and the lack of public transport. These combined will inevitably result in the detrimental impact on the elderly, disabled and vulnerable residents residing in the area next to the development.

I would urge that the 130 plus written letters of objection from local residents which have been submitted, as well as the clear objections from elected representatives voicing their constituents' concerns, are given due consideration when this is taken to Committee.

7.7 Julie Morgan AM states:

I have been contacted by many of my constituents who live near the proposed development at Highfields off Allensbank Road in the Heath area of Cardiff. They have expressed a great deal of concern about this application and I share many of their concerns.

Density

The proposal is for a total of 42 units, made up of 12 x two bedroom houses, 20 x two bedroom flats for the open market and 10 affordable flats, (5 of them one bedroom and 5 of them two bedroom). This means there would be properties with a total of 79 bedrooms at that location. This seems to be an extremely high number for a relatively small area. I understand that this development is part of a series of developments proposed by the Council and Wates Residential and that apart from one other development; this one has by far the highest density, at 72.07 DPH. The only one with a higher density is at Briardene, which is a completely different type of location, not surrounded by bungalows for the elderly and

disabled. Also, the site at Walker House has only 16 units, even though it is of a very similar size to Highfields, meaning that the latter would be almost three times as densely occupied as the Walker House location.

The area around the proposed Highfields development is occupied by many elderly and disabled people who live in bungalows and currently enjoy a level of privacy and quiet which is likely to be greatly disturbed by this proposed development. This is naturally causing them a great deal of anxiety and I am very concerned about the detrimental impact this proposal will have on their enjoyment of their homes.

Height

Another concern about this particular development is the height of the block of flats which will be built close to the existing bungalows. The block is proposed to be three storeys high, which is out of keeping with the area and will be overbearing for those living next to it. The current bungalow residents, many of whom have difficulty going out, are very upset that their outlook will be completely changed and they will be overlooked by the new residents.

Traffic and Parking

Parking in cities like Cardiff is a serious problem and one about which many people are very concerned. In the area of this proposed development, which is close to UHW, it is already clear that parking is an issue and there is considerable concern that a development of this density will add to that problem. With a total of 42 homes with 79 bedrooms in the development, the provision of 1 parking space per unit and 4 for visitors seems insufficient and likely to lead to extra pressure on the limited parking spaces in the local area. There will also be a considerable increase in traffic using Allensbank Road, which is already very busy, particularly at rush hour. There is therefore a consequent increased risk of accidents in that area.

Public Transport

Unfortunately public transport in the area is very limited, with only the number 86 bus running along Allensbank Road, once an hour and only until 6pm on weekdays. The bus is less frequent on Saturdays and does not operate on Sundays. Trains from Heath Halt are extremely busy at peak times, with many passengers being unable to get a seat. Heath High level is also very difficult to access for elderly or disabled people as it is at the top of a long flight of steps.

In view of all these factors, I ask that at the very least, consideration be given to a reduction in the density and height of this proposed development.

7.8 David Melding AM states:-

I have been contacted by the Heath Residents Association. They have raised concerns around overlooking and privacy for adjacent residents in Heathmead, many of whom are classed as vulnerable, elderly or disabled. The issues of the height and proximity at this boundary are a key concern and therefore I hope that the planning committee will consider a site visit to see this aspect first hand before reaching a decision.

7.9 The Heath Residents Association objects for the following reasons:-

- 1) The density of the 42 units, comprising of 12 houses and 30 flats, is far too high. It is more than double the density of surrounding residential properties and nearly three times the density of other similar Cardiff Living projects.
- 2) The 3 storey single block of 30 flats will have a severe and detrimental impact on the lives of the elderly, disabled and vulnerable Council tenants living in the single storey bungalows close by.
- 3) The buildings are not in keeping or sympathetic to the characteristics of the surrounding residential area. All are one and two storey residential dwellings.
- 4) The Passivhaus, low energy design will not have the correct effect and is completely out of orientation. The buildings are designed and planned in the wrong direction to benefit from sunlight trajectory. Instead of maximising sunlight from a south/north facing line the buildings run east/west.
- 5) Traffic will be significantly increased in an area that has already been identified by the Council as a high accident zone.
- 6) Existing UHW on street car parking problems for residents will be made much worse by the density of this site, with a total of only 46 car parking space for the 42 units and visitors, most of which are two bedroom units. Only 10 flats will be Council owned and affordable. What elderly resident would want to live on the 3rd floor with no lifts, no shops and very limited bus service? An estimated 100 cars will be generated by this majority open market development so close to the largest university hospital in Wales. It is anticipated that most will therefore be bought and rented out, making no contribution to Passivhaus Living or of benefit to the local community
- 7) No Section 106 contributions are being suggested that could assist with implementing to Resident & Restricted parking, which is strongly supported by residents who cannot park or have visitors, builders, deliveries or family or carers park near their properties anytime during the day Mon-Fri.
- 8) To make Passivhaus Living (green low energy/carbon style living) matters worse, there is a very limited bus service along Allensbank Rd/ Heath Park Avenue, so most will be car dependant purchasers. The existing service is hourly most days and ends early evening. The consultation document has many errors, for example, it includes a school bus service as a public transport option and nearby shops are half a mile away in Whitchurch Rd. In fact the nearest regular bus service is on Lake Road West, which is hundreds of metres away and up a very steep incline (Highfield Rd).
- 9) The Pre Consultation was not carried out to a good or acceptable standard, using the minimum 28 day period and held during the summer holiday period. Only the minimum number of directly adjacent residents were contacted, despite the impact on those in nearby properties who were unaware of the proposals. Some residents had multiple letters, some had none. Elderly and disabled residents were expected to read the single notice displayed on the entrance gate and visit Wilcox House in Cardiff Bay to view plans. Most do not have access to the internet and some do not have the physical skills or equipment to view online.
- 10) Despite the above, more than 130 written letters of objection from local residents have been submitted during the pre-consultation exercise, showing that residents feel strongly against this design and density being proposed. Our online petition currently has 320 signatures: <http://chn.ge/2qQZBi>
- 11) The impact on the Vision 21 Oaks Garden Centre, which has served as an adult learning centre for up to 35 students per week and for more than 20 years, will be severe and could close from being enclosed and "hidden away" from public view by this over development.

- 12) All elected Members, except one, have objected to this application. This includes both AM and MP for Cardiff North and two of the three local Councillors. The HRA are not against housing on this site but do feel 42 units and the single 3 storey block is completely out of character with all existing neighbouring residential buildings. The only other 3 storey building nearby is Heath Park Court, where the first floor is below natural ground level, having been built on an old railway siding. This building appears to be two storey from most directions of other residential properties.

In summary, we strongly object to these proposals on the grounds of poor design, high density, increased traffic and lack of public transport links and local amenities and the severe impact this proposal will have on our elderly, disabled and vulnerable residents living and working next to the proposed development.

- 7.10 (a) The Chair of the Heath Residents Association has submitted a lengthy letter of objection which is available for inspection on the planning page of the Council website. I have summarised his objections as follows:-

Need to safeguard elderly and vulnerable who will live in shadow of and overlooked from the apartments, which is like the Berlin Wall. The use of rear balconies will result in noise and disturbance adversely affecting residents of the bungalows.

Community should be more involved in Council decisions.

The apartments are 3 storeys high contrary to SPG in relation to tall buildings.

The overall density of this site has a DPH of 72.07, which is not at all in keeping with the existing area or with other developments that the Council and Wates are entering into. Many people believe that the reason for this to make the site viable because of Passivhaus Energy.

The decision in 2012 has been set aside.

The buildings are not in keeping or sympathetic to the characteristics of the surrounding residential area. There are no other flats of a similar appearance in the area. The flats will physically segregate the community, dividing Heathmead from the mews houses to be built and Allensbank Road like Northern Ireland's Peace Wall.

Not orientated correctly for Passivhaus. Passivhaus will make the units more expensive.

Train services run from Heath Halt but during peak hours the trains are packed before they get to the Halt. There is also an accessibility issue.

There is no indication how children, other than those attending the Welsh School, will travel to school

Traffic in the area is already a big problem and this area has recently been identified as an accident black spot. The increase in traffic will make this worse.

There are existing parking problems in the area and the parking proposed is insufficient.

School places are at a premium and appointments in surgeries are difficult to organise at short notice.

The Oaks garden centre requires a safe environment and this will change.

The process of the pre-application consultation exercise is criticised.

The Council as co-developers should withdraw this application.

There should be a site visit before the application is considered.

More bungalows are required not apartments.

- (b) The Chair also points out that since the submission of the application the bus schedule has altered so that the early morning buses cease to run along Heath

Park Avenue/Allensbank Road. The first bus south is now at 9.17 am and the first bus north at 8.21 am. This would mean that anybody in employment would not be able to get to work in time and any child attending senior schools to the north would not arrive on time.

In addition the buses will cease to run every hour and will only run every 90 minutes. It must be noted that there is no east/ west service so the timings of the bus would also affect those requiring to link with such services.

On Saturdays it gets worse in that the first bus south is 9.17am and the first bus north is 9.15am. All services cease at around 6pm. There is no service at all on Sundays.

In summary he says that the bus service is totally inadequate and therefore this is yet another reason why this development as it stands should not go ahead, there being no chance of the Council's policy of 50/50 transport being achieved.

- 7.11 The objections received in writing cover the following matters:-
- Insufficient parking, aggravating existing parking problems (allegedly caused by UHW) and no resident parking permit controls.
 - Building too high and too close to adjoining bungalows, resulting in overlooking, reducing light and restricting views
 - Block light and view from houses on Allensbank Road
 - Density too high
 - Increase traffic, making road noise louder, higher probability of traffic congestion, hamper emergency vehicles travelling to the hospital, increase risk of road accidents at accident blackspot and increase air pollution
 - Added pressure on bus and train services
 - Not in keeping with style of housing throughout Allensbank Road
 - Architectural quality is nil
 - Increased vulnerability of adjoining elderly and disabled residents in bungalows
 - Noise and disturbance
 - Site should be a recreational area.
 - Devalue nearby properties
 - Adverse effect on people attending the Oaks, no details on what will be done to stop residents using their parking spaces, should have separate vehicular access from Allensbank Road and disabled will be put off using the Oaks as it will be less safe.
 - Access to the Garden Centre needs to be maintained during construction period.
 - Water pressure will be reduced
 - Strain on electricity services
 - The second vehicular access has been unused for many years and would cause traffic problems for residents of Heathmead
 - Changing the local demographic
 - Lack of affordable housing in the scheme.
 - The proposal is a moneymaking ploy to make the most profit.
 - Feel this is all just excuses in the pretence of helping the homeless.
 - No lift in apartment for older people
 - Site should be used for shops or hospital parking.
- 7.12 One objector has submitted a lengthy letter of objection which can be viewed on the Council's website. In summary he has raised the following additional issues:-
- The site was not included in the LDP and has not followed the normal design process and checks and balances
 - The site design has not fulfilled the requirements of PPW
 - The design is not in accordance with many of the requirements of TAN 12.

The "Site and Context Analysis" guide has not been followed when evaluating the constraints for this site

The Wales Spatial Plan has not been followed for this site with the wellbeing of some residents being ignored

The Manual for Streets has not been followed when designing the site

The Tall Building SPG has not been applied to the site design

The principals of Cardiff and Wales Well Being policy has been ignored

The design and access statement is inappropriate and not based on a proper context analysis of the area.

The Passivhaus standard will make the properties more expensive in an already expensive area

The Oaks Garden Centre has had parking spaces reduced and should be entirely protected

The density of the site is twice that of existing surrounding areas and three times that of other proposed sites.

The terraced mews are not in keeping with houses opposite, the mews having inappropriate frontages

Existing tree lined views and vistas along Allensbank Road will be destroyed

The consultation process was inadequate with minimum letter drop techniques employed throughout

PAC reports and full design plans and documents are submitted with days of the closure of the consultation period clearly demonstrating that the designs were complete before the statutory consultations took place.

Residents were frequently encouraged by Cardiff County Council to submit objections at the planning application stage as opposed to the consultation stages

No clear detail on how these flats and mews would be sold, to whom at and what price has been made available

PAC report defines meeting and discussions that have shaped the design, with Highways and planning officers yet no minutes or summaries of meetings have been provided for public scrutiny.

Due to the high density, from the transport statement there will be 145 vehicle trips per day minimum from the site which will significantly increase the current traffic pollution and have a detrimental health affect in the area

No analysis on the possible additional pressure on schools has been prepared.

The Council identified but opted not to consult with recognised local voluntary housing association in direct conflict with PPW in spirit and in fact, demonstrating the "them vs us" approach that Cardiff County Council and Wates have taken throughout this planning process.

7.13 One letter of objection is in respect of the publicity arrangements. In summary it says that it forces people to object on line who may not have access to a computer. It also questions how people on mobility scooters or disabled can inspect the site notices displayed. A pre-application consultation site notice is still displayed which causes confusion. This letter can be reproduced in full if Members require.

7.14 One resident has submitted a lengthy letter of criticism of the Passivhaus element of the proposal which is available for inspection in full on the Council website. In summary he says that:-

The orientation of the units is incorrect as it does not maximise solar gain

The backdoor to the houses will be most used so should have a hallway space

Passivhaus is being used to justify the size of the apartments

Future residents may not comply with operating procedures for Passivhaus.

The same local resident has further disputed additional information submitted by the applicant's agent regarding the Passivhaus element of the scheme. The full contents of the letter are available for inspection in full on the Council website. He maintains his criticisms and considers the application cannot be justified on grounds of Passivhaus.

- 7.15 A petition signed by 48 local residents and 2 from Rhiwbina objecting on the grounds that density of building is too high and significantly exceeds the density in the existing area. The 3 storey flats which will overshadow the bungalows in Heathmead currently occupied by elderly and infirm residents. The flats are not in keeping with existing buildings or character with no precedence of large blocks of flats in the area. Insufficient parking spaces and high density will add pressure to current well documented local parking issues. High density will add pressure to current traffic issues in an area with documented history of traffic accidents resulting in the recent traffic calming measures being installed. The lack of application of the Tall Building Policy that would require additional and different surveys and compliance requirements. The potential impact on the garden centre.
- 7.16 Another petition signed by 20 local residents and 2 other Cardiff residents object to the proposal for the block of flats overlooking the existing Heathmead area.
- 7.17 An electronic petition has been submitted with 316 names and no full addresses. 127 are from Cardiff and the remainder are primarily from elsewhere in the UK with some from the USA, Singapore, Germany, Portugal, Greece and Cyprus. The petition says "Help Stop the Highfields Development Cardiff". 71 of those who signed the petition made comments as to why they were objecting. These comments are in line with the objections described above. One of the comments was along the lines that if this development was allowed that an impression of corruption would be created.
- 7.18 During the processing of this application amendments were received covering access arrangements, appearance of terraced houses, landscaping, boundary detailing at junction of Allensbank Road/Heathmead, an additional visitor parking space, reposition bin/cycle store, size of balconies and side windows to south elevation of apartments. On 11/01/2018 a letter notifying those who were originally notified of the application and those who submitted letters of objection/comments were notified of the amendments received and given 14 days to respond.
- 7.19 (a) An objection to the access has been submitted. The objector says that I believe the junction should be classed as a Major/Minor road junction based on a 30 mph speed, in accordance with TD4295. The northern radius and northern visibility splay is inadequate.
(b) An objector says the amendment to the design of the house will affect the passivhaus design and inadequate time to respond to notification of amendments.
(c) An objector says that in May 2016 he was told by a Housing officer that the Heathmead vehicular access was required to facilitate refuse vehicular movements and is now no longer provided.
- 7.20 Two other objections have been received which reiterate some of the objections previously described in paragraph 7.10.

8. ANALYSIS

8.1 The application site is owned by the Council and was formerly occupied by a day centre for people with physical disabilities. The previous building has been demolished and the site cleared. The site is a brownfield site within the settlement boundary of Cardiff. The principle of the development of this site for residential purposes raises no land use policy considerations.

8.2 The applicant states that:-

The site area allows an opportunity for development of different scales with enough space to accommodate buildings which are larger than those of the immediate context, without unduly affecting neighbouring properties. There is also an opportunity to 'fill the gap' in development along Allensbank Road, bringing an active frontage to this currently redundant space.

8.3 In respect of the issues associated with this application, including LDP policies and SPGs, and the objections/concerns raised I would make the following comments:-

Density

8.4 In terms of density the proposal is a high density development that makes efficient use of land which in turn helps reduce pressure for the future development of greenfield sites. It is acknowledged that the development is at a higher density than the nearby 1920 – 1950 housing. Heathmead which was approved in 1982 is at a higher density than the earlier housing development in the area but is at a lower density than that currently proposed. The Heathmead development would have been assessed against the policies and circumstances appertaining some 35 years ago.

8.5 The development is for a higher number of dwelling units than that specified in the 2012 outline planning application. However, option 1 for 27 dwellings in application 12/00213/DCO would have comprised 97 bedrooms compared to the 79 bedrooms in the current application.

8.6 Policy KP5 (ix) of the LDP promotes the efficient use of land developing at highest practicable densities.

8.7 Simply based on the number of dwelling units, and not the amount of accommodation provided within each dwelling, this application has a density of 60 dwellings per ha. By comparison the approved Briardene development is to be developed at a density of 75 dwellings per ha and the existing Heathmead development was constructed at a density of approximately 47 dwellings per ha. The proximity of a park was taken into account in determining the Briardene application as a few of the proposed dwellings had insufficient amenity space to meet the Council's guidelines.

8.8 The Cardiff Residential Design Guide SPG asks "Does the development make efficient use of land and provide homes at a reasonable density?" The SPG states that:-

"The vitality and viability of neighbourhood centres and public transport services are dependent upon having a good number of potential customers within their

catchment areas. Low density forms of development can reduce their viability, and often result in less sustainable forms of development which can't be readily reached by active travel modes. In order to support the vitality and viability of local shops and services we will welcome designs that can increase development densities within walking distance, where they also conform to other guidance presented here.

In addition, variations in the density of development can allow for greater variety in the spatial and visual character of a scheme which can enhance the townscape and help people find their way around. There is no relationship between development density and the ability of a scheme to provide adequate green spaces or meet amenity standards, although the form of green space might go from providing private to more shared forms of garden or open space. We will always welcome interesting approaches to providing amenity spaces."

- 8.9 The Cardiff Residential Design Guide SPG shows 4 examples of density. Radyr Sidings 29 dph, Phoenix Way 66 dph, Ninian Park 47 dph and Porth Teigr 70 dph. When considering density it is not simply the number of units that is significant but the consequences on matters such as the amenity of adjoining and future residents, appearance, access, parking, overshadowing, etc.(These matters will be assessed later in this Section of the report).

Amenity space for future occupants

- 8.10 The Residential Design Guide SPG identifies amenity space areas for houses and apartments. In this case there are good sized gardens that are each 121 sq m in area to the ground floor rear open market apartments that exceed the 50 sq m required for houses in SPG.
- 8.11 A communal area for the affordable housing is 232 sq m in area which again exceeds the Council's guidelines. (The SPG specifies for communal amenity areas 75 sq m for up to 5 units with an additional 10 sq m for each additional unit (ie 125 sq m)).
- 8.12 The SPG states that balconies a minimum of 5 sq m will need to be provided for apartments without access to communal gardens or suitable public green spaces in the vicinity. The upper floor apartments have terrace areas 17sqm in area which exceeds the guidelines. The other 12 balconies to the open market apartments are approximately 3.6 sq m in area. (The original submission showed balconies 3 sq m in area). The applicant says that the revised scheme will now accommodate a table and chair.
- 8.13 The front ground floor open market apartments have a 10m strip of shrub planting in front of them
- 8.14 The applicant says that the gardens to the houses typically measure 48.5 sq m, marginally below the 50 sq m identified in the SPG. The SPG says that standards will always be used flexibly if a design solution justifies it. To the rear of the houses is a landscaping strip which if included within curtilages increases the amenity area, but this landscaping contributes to the character of the shared space and as such flexibility to the standards can be justified.
- 8.15 In view of the proximity of Heath Park and Roath Park Lake, that the majority of apartments exceeds the amenity space requirements and that there are only

limited marginal deficiencies in the amenity area for some of the proposed units (there are also incidental areas of amenity space primarily used for landscaping purposes within the site) then the overall amenity space provision is considered acceptable

Parking

- 8.16 LDP Policy KP8 Sustainable Transport states in part that:-
Development in Cardiff will be integrated with transport infrastructure and services in order to:
- (i) Achieve the target of a 50:50 modal split between journeys by car and journeys by walking, cycling and public transport.
 - (ii) Reduce travel demand and dependence on the car;
 - (iii) Enable and maximise use of sustainable and active modes of transport;

- 8.17 Supporting paragraphs of this Policy states:-

4.106 For Cardiff to accommodate the planned levels of growth, existing and future residents will need to be far less reliant on the private car. Therefore, ensuring that more everyday journeys are undertaken by sustainable modes of transport, walking, cycling and public transport, will be essential.

4.107 The location and form of developments are major determinants of the distance people travel, the routes they take and the modes of transport they choose. Much of the growth in car travel in recent decades can be attributed to developments which have been poorly integrated with the transport network. Integration of land use and transport provision can help to manage travel demand, avoid developments which are car dependent and make it easier to facilitate movements by sustainable modes.

4.108 The purpose of this Key Policy, therefore, is to ensure that developments are properly integrated with the transport infrastructure necessary to make developments accessible by sustainable travel modes and achieve a necessary shift away from car-based travel.

8.18 LDP Policy T1 encourages walking and cycling to minimise car use and support the Council in fulfilling its legal requirement under the Active Travel (Wales) Act 2013

8.19 LDP Policy T5 also seeks to reduce reliance on the private car in line with national planning policies and strategic transport objectives. This proposal also provides one cycle space per apartment and there is space within the curtilage of each house for secure cycle storage.

8.20 The Council's SPG on Parking provides parking standards for non central area development, in this case. The minimum parking standard requires 29 spaces. This application identifies 1 space per dwelling and 5 visitor spaces, a total of 47 spaces. This exceeds the minimum standard by over 50%.

8.21 The proposal accords with the Council's planning policies and SPG. In this case bus and train services, leisure facilities, a school and shop are all within reasonable walking distances. As the site is not in a peripheral location, where reliance on private cars may be greater, and the minimum parking standards are exceeded it is considered that a refusal on grounds of inadequate parking could not be sustained.

8.22 Furthermore the Local Planning Authority has determined applications in line with the Council's Transportation policies and Parking guidelines for example:-

Committee recently approved a HPP scheme at Briardene for 30 open market units and 9 affordable units each with one parking space per unit and no visitor parking (17/01691/MJR).

A HPP residential scheme at Llandudno Road for 16 dwellings (11 open market and 5 affordable units) was approved in October 2017 with one car parking space per dwelling and no visitor parking (17/01681/MJR).

- 8.23 The previous outline application on this site indicated 3 and 4 bedroom units. Such units require more parking to meet minimum car parking standards than the 1 and 2 bedroom units that are now proposed.
- 8.24 Objectors have stated that on street parking problems in the area are caused because of people using the nearby hospital. In view of the site's sustainable location, car and cycle parking to be provided within the application site and the existing full use of on street parking on nearby roads it is not considered this situation could be further exacerbated by this development. Furthermore Government advice on Planning Obligations states that "Developers should not be expected to pay for facilities which are needed solely in order to resolve existing deficiencies..." Therefore, it would not be appropriate in this case to seek a S106 Agreement to address an existing problem. In addition the provision of resident parking permit arrangements rests with the Highway Authority.

Adverse effect on adjoining residents to the east (Heathmead)

- 8.25 Objectors claim that residents of the adjoining bungalows will be adversely affected by the development. The nearest bungalows are a minimum of 22m from the apartments, 21m from the modest sized balconies to the first floor apartments, 28.5m from the nearest part of the second floor apartments and 26m from the second floor roof terraces. The rear of the apartments varies from approximately 11.3 – 11.8m distant from the boundary with the adjoining bungalows.
- 8.26 The Council's SPG on Residential Design requires at least 21m between facing windows to habitable rooms on the private side of a development and this is exceeded between the rear of the proposed apartments and the rear of the adjoining bungalows. Another requirement is that the minimum overlooking distance from a rear window above the ground floor to a private garden of a facing dwelling should be at least 10.5m. This distance is also exceeded in this case.
- 8.27 The SPG provides for a 25 degree rule in identifying whether light to existing dwellings is adversely affected. This says that light to a room that provides daytime living space will be obstructed if a line projecting from the centre of the relevant window infringes 25 degrees to the horizontal. In this case neither the two nor three storey element of the apartments comes close to infringing that rule in relation to the existing bungalows.
- 8.28 The development exceeds the standards laid down within the Council's SPG in respect of privacy and daylighting in relation to the adjoining bungalows, thereby safeguarding neighbours' amenities. Furthermore existing trees along this boundary are to be retained and 7 additional trees planted. This will help further reduce and soften any impact the apartments may have on the occupiers of adjoining properties.
- 8.29 The bungalows at the northern end of Heathmead are in an elevated position that in part overlooks the car parking associated with the Oaks. The parking area

serving the Oaks will be slightly rejigged and the adjoining bungalows will overlook this parking area but this will not materially affect the existing relationship.

- 8.30 Having regard to the above the overall positioning and scale of the apartments in relation to adjoining dwellings are considered to be acceptable.

Contrary to Tall Buildings SPG

- 8.31 The Tall Buildings SPG states that:

For the purposes of this guidance, a tall building is normally considered to be:

Within the city centre and Cardiff bay: 8 storeys+ or circa 25m+

Outside the city centre: Buildings which are double or more than double height of surrounding properties or significantly taller in terms of actual height and number of floors

- 8.32 The Tall Building SPG is not applicable in this case as the dwellings in the surrounding area comprise both single and two storey dwellings and the proposed apartments are part two and part three storey as described earlier in this report. Furthermore, the overall height of the adjoining bungalows is 5.6m and the rear wall of the apartments is 7.6m high and the front wall of the apartments is 10.2m. The apartment building, for both the two and three storey element, is less than double the height of the adjoining bungalows and only 2m higher than the height of the bungalow at its closest point to the apartment building.

Vulnerability of adjoining residents

- 8.33 Many of the objectors have expressed concern about this issue. It is claimed that residents of the adjoining bungalows would become more vulnerable as a result of this development.
- 8.34 Currently the application site up to the rear boundary of the bungalows is readily accessible. There is a pedestrian route through from the application site into Heathmead (the proposal will close off this route).
- 8.35 Most of the existing bungalows are enclosed by a hit and miss fence along the rear boundary although some have been reinforced by solid fencing. There is currently the opportunity to observe some of the rears of the bungalows through the hit and miss fencing from the unenclosed application site. A 1.8m high fence should be required to be erected along the boundary between the bungalows and proposed apartments (proposed condition 21, which will improve security to most of the bungalows that adjoin the site. The windows in the apartments will increase passive surveillance over the land to the rear of the bungalows.
- 8.36. As a result of the removal of the pedestrian route through the site into Heathmead, the erection of a 1.8m high close boarded boundary fence and increased passive surveillance will contribute to the security of residents of the bungalows.
- 8.37 The applicant has confirmed that SBD compliant access control systems to the apartments will be installed. This addresses the one issue identified by the Police who otherwise have no objection. The Police have submitted no objection in respect of the vulnerability of existing residents.

- 8.38 The Housing Development Manager advises that a letter of invitation to attend the Heathmead residents meeting was hand delivered to all residents of the sheltered housing scheme. The following is a note of the meeting:
Heathmead Residents meeting with tenants participation & Cllr Thorne, Cabinet member for Housing & Communities.
Meeting held on: 6th November 2017
In attendance: Cllr Thorne, D. Jaques & Lianne Barrett.
2 residents at the meeting and we saw a further 3 residents in their homes. Therefore 5 residents were spoken to in total.

The 2 residents at the meeting were in full support of the scheme and complained about the contact they had had from the Heath Residents Association. They had some queries about the impact the scheme will have on pedestrian access from the bungalows at Heathmead through to the main road. I was able to confirm that access will be maintained throughout the development process and wouldn't alter once the scheme is complete (This was in response to concerns that the footpaths on Heathmead Road will be closed during construction which is not the case).

The 3 residents that we spoke to individually all faced into the development site and have concerns regarding the blocking of light from the proposed development into their gardens and into their properties, additional traffic, potential noise pollution from the new residents and having a lack of privacy from residents from the new development overlooking their habitable rooms. I talked through the scheme with them in detail and explained that the 3rd storey of the proposed development only covers the front elevation of the building and would not look over their property. I also explained that the distances required by planning policy from building to building was being met. We also talked about screening the development by using trees and that a 1.8m high fence line would be in place

- 8.39 The Housing Officer confirmed that there is no warden at Heathmead. An objector had claimed there was one.
- 8.40 Section 17(1) of the Crime and Disorder Act 1998 imposes a duty on the Local Authority to exercise its various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all that it reasonably can to prevent, crime and disorder in its area. This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable increase in crime and disorder as a result of the proposed decision.

Adverse effect on amenities of residents to the west (Allensbank Road)

- 8.41 The dwellings on the opposite side of Allensbank Road are some 23m distant from the proposed houses, separated by a public highway with pavements on either side. There is no infringement of the Council's privacy standards or the 25 degree rule in this case. For many new developments the acceptable distance between the fronts of different dwellings is significantly closer than that proposed in this scheme.
- 8.42 There is no right of a view over another party's private land.

Access

- 8.43 Since the submission of the application and following discussions with the Transportation Officer the vehicular access from Allensbank Road has been

amended by widening it to 5.5m with 2.25m pavements either side. The second vehicular access to Heathmead has been omitted. The Transportation Officer has no objection to the amended layout subject to proposed conditions 19 and 20.

8.44 The applicant states inter alia that:

It was discussed and agreed during the development process, that locating any form of entrance near the northern end of the site would be problematic. The curve of Allensbank Road results in an obstructed view of approaching vehicles, exacerbated by the tall stone wall in front of the Oaks. There is also the proximity of Highfield Road to consider. In fact, it was these concerns which led us to relocate the entrance to the south in the first place.

Vehicular access will be shared with the Oaks. Two signs within the site will provide direction to the garden centre parking area. There will be a separate parking area of 14 spaces serving the Oaks, who will have sufficient parking for their needs, and with a separate pedestrian access leading from this car park into the Oaks.

8.45 The Housing Officer has advised that parking provision for the Oaks forms part of a Community Asset Transfer. It was a condition that parking to the Oaks be provided when Housing bought the land from Estates. Fourteen was the number of spaces agreed following a period of dialogue with the garden centre, and wider consultation with the local community. Broadly, it reflects the current parking capacity, and is sufficient to accommodate peak demand during the weekend (and in spring).

8.46 The proposed access is 20m south of the existing vehicular access which will improve visibility to the north. Recent traffic calming measures, by the introduction of raised platforms on Allensbank Road at its junction with Highfield Road to the north and the pedestrian crossing to the south, contributes to improved highway safety measures in the vicinity of the site access.

8.47 The Transport Statement estimates, by reference to the TRICS database, that there will be 145 vehicle movements a day. This equates to one vehicle movement every 10 minutes over a 24 hour period or one vehicle movement every 5 minutes during peak hours. This would not result in a significant increase in traffic movements along Allensbank Road. It should be noted that there was a certain number of traffic movements associated with the previous use of the site, even though the number of movements is believed to have been less.

8.48 The overall configuration of the access, with the central route towards rear parking to the frontage homes, and the shared space route to parking for the apartments and garden centre is considered to be acceptable. There is a separate footpath for vulnerable users. The shared space area is reasonably short and tight encouraging slow driving speeds. It will be visually contained by trees/landscaping.

Second vehicular access onto Heathmead.

8.49 The applicant has amended his scheme since submission, following discussions with officers, and widened the access from Allensbank Road and omitted the vehicular access to Heathmead. A proposed pedestrian access will instead be retained to Heathmead. This rearrangement has enabled the creation of one additional visitor space. It is considered that the objection raised to the use of a vehicular access onto Heathmead has now been addressed.

Pressure on public transport

- 8.50 In respect of pressure on bus and train services the proposal is for 42 dwellings in a ward of over 12,000 people. It is not considered that the proposal would make a significant difference to the capacity of bus and train services in this location. The proximity of bus and train services reflects the sustainable location of the development which helps reduce reliance on private vehicle movements. A covered cycle space will be provided for each apartment and there is space available within the curtilage of each house to securely keep a bicycle.
- 8.51 The Institute of Highways and Transportation issued guidelines for “Providing for Journeys on Foot” and identifies in table 3.2 suggested acceptable walking distances of desirable 400m, acceptable 800m and preferred maximum 1200m. Recreational areas and public transport services are all within the desirable or acceptable guidelines and Allensbank school and two public houses are within the preferred maximum range.
- 8.52 There is a bus stop 50 m north of the application site and another 125m south of the site both on the eastern side of Allensbank Road. There is a bus stop on the western side of Allensbank Road opposite the Oaks. The bus service is the 86. There is a more frequent bus service to and from the bus stops at Heath hospital to various parts of Cardiff, which is 500m distant from the application site. The bus stops at Roath Park Lake are 460m distant. The Heath railway station is approximately 700m north of the application site.
- 8.53 There is a corner shop on the western side of Allensbank Road 40m from the site next to a zebra crossing. The Heath Hospital is close to the application site, Heath Park is approximately 100m distant, Highfields allotments is 350m distant, Roath Park Lake is 450 m distant and the site is 1Km from Allensbank Primary School.
- 8.54 It is considered that the proximity of the afore-mentioned facilities contributes towards the development being able to comply with LDP policies KP8, T1 and T5.

Adverse effect on users of the Oaks.

- 8.55 In respect of the relationship with the Oaks the Housing Officer advises me that: The garden centre will have 14 formal spaces but if the turning area and delivery space is used there will be space for 18 cars. This is similar to what they have now. The area identified has been discussed in great detail with the centre and the space will be managed by them. They can put a barrier up if they want to. An accessible ramp is to be provided to the house.

A separate access was not an option because an entrance couldn't have been created closer to the Highfields road junction. Also another entrance would mean less units which would mean the site would not be viable.

In addition to signage within the application site brown signs are to be erected along Allensbank Road on both approaches to the garden centre. Wates and the Council will continue to work closely with the centre. Wates have given them funding for projects as well. .

- 8.56 There is a direct pedestrian link from the Oaks parking area into the garden centre. The pedestrian link has been widened to 2m following discussions with officers. There has been no objection from the Police or Transportation Officer in respect of

the safety of persons using the Oaks. The development will increase passive surveillance of the Oaks car park.

8.57 Notwithstanding issues raised about the impact on the Oaks It is not considered that the development materially affects users of the adjoining Oaks as access and parking will continue to be provided. It is necessary to ensure that a vehicular access be retained to the Garden centre during the construction period and proposed conditions 14 and 15 are designed to address this.

8.58 No objection has been received from the Oaks.

Out of character with area and Poor architectural quality

8.59 The applicant appears to have generally taken on board the advice of the DCfW in the formulation of this application. Notwithstanding this advice and pre-application discussions with the applicant there have been further discussions with officers during the processing of this application to significantly improve the appearance of the houses visible from the public highway.

8.60 Much of the development in the area consists of pleasant pre-war semi detached two storey housing, which could not be recreated on the application site. The two storey terraced housing is sited in a similar manner to the existing terrace housing to the south. The materials to be used are similar to those used at Heathmead. Whilst there are differences between existing and proposed dwellings in this area the existing buildings are not listed, nor fall within a Conservation area or have any special character/design status. The building that was on this site may have been considered out of keeping with the character of the area

8.61 The 2 storey white render dwelling houses comprise two matching terraces stepped down from north to south with reasonably simple elevations. The front elevations include brick detailing in the form of a plinth and frame to the door, otherwise the character is paired back. The white render is used nearby in both Heathmead and Allensbank Road.

8.62 The front elevation of the houses as originally submitted was considered rather dull with limited street appeal. Following discussions with officers the appearance of the houses has been enhanced by the following measures:-

First floor windows are larger and now serve two bedrooms rather than two bathroom windows and a landing window. The interior of the dwellings has been rearranged to accommodate this improvement.

Addition of chimneys to each house

Inclusion of rainwater downpipe between front doors to define individual properties
Introduction of deeper eaves rather than close clipped eaves which were previously proposed to add some depth and shadow to the elevation.

Increased the brick surround to the front door.

A first floor window has been added to the gable elevation with a brick slip surround.

8.63 The configuration of houses onto Allensbank Road creates a good frontage, slightly set back from the pavement, behind the retained stone wall with any gaps reinstated with stone walls. The set back is just over a metre. It is similar to set backs a short distance to the south. The houses will introduce a bit more variety into the road in terms of spatial character.

- 8.64 The applicant originally showed the end elevations to the houses fronting the central access route having small windows on the back of pavements, and given the suggested wall depths there would not have been much view of the access road which would not have felt very overlooked. The SPG expects corner buildings to front more convincingly all street frontages and following discussions with officers the applicant has added an extra window to both end houses to address this issue
- 8.65 The overall form and design of the apartment building is considered acceptable as the fenestration is well composed, and the block has some variety created by the material bands and balconies. In terms of public views the apartment building will be glimpsed through the central access road into the site and seen from an angled view into the site from the southern pedestrian link near the entrance to Heathmead.
- 8.66 Tan 12 Design identifies the five elements that contribute to good design which are Community Safety, Environmental Sustainability, Movement, Access and Character. It is considered that this scheme satisfactorily addresses these five elements.

DAS inadequate

- 8.67 The government says that the content of a DAS will vary depending on the project, as a minimum it must explain the design principles and concepts that have been applied to the development and how issues relating to access to the development have been dwelt with.
- 8.68 The DAS included the following sections: introduction, background, passivhaus, physical context, opportunities and constraints, policy context, amount, layout, site layout dwellings, scale, appearance, landscaping strategy, community safety and security, access and movement, environment sustainability, consultation and pre application advice, maintenance and ownership strategy. The DAS is considered to be fit for purpose. The submitted details that accompany this application as amended are considered sufficient to enable the Authority to properly assess this submission.

Noise and disturbance

- 8.69 An objection has been made in respect of noise and disturbance during construction and upon occupation. Construction noise and associated disturbance with any form of development on this site will only be for a temporary period. Once the dwellings are occupied there would be the normal residential noise associated with the occupation of modest sized dwellings.
- 8.70 Conditions 21- 24 and Recommendation 3 address in part some of the issues covered by this objection.

Site should be a recreational area or used for some other purpose.

- 8.71 There is no Council proposal that this site should be developed as a recreational area. Heath Park and Roath Park Lake are both relatively close to the application site and are of a good size. Most of the surrounding properties have rear gardens varying from 10-16m in length.

8.72 Whatever alternative uses objectors may propose each planning application has to be judged as submitted.

Devalue nearby properties

8.73 Paragraph 3.1.4 of PPG Wales edition 9 states inter alia that “The planning system does not exist to protect the private interests of one person against the activities of another. Proposals should be considered in terms of their effect on the amenity and existing use of land and buildings in the public interest.” The claim that this development would devalue existing properties is not a material planning consideration.

Water pressure will be reduced

8.74 In terms of water pressure Welsh Water has raised no concerns in respect of water supply and confirms that a water supply can be made available to serve this proposed development. No evidence has been submitted to substantiate an existing or future problem. The previous use on this site was provided with a water supply.

Strain on electricity services

8.75 One objector has referred to a strain on the electricity supply but has provided no evidence to substantiate such a concern. The energy provider will have to ensure adequacy of supply with the new substation.

Changing the local demographic

8.76 The 2011 census describes the number of people in Cardiff that are resident in purpose built apartments and in other forms of accommodation. For ease of understanding this has been converted to a percentage by age group compared to the total population which is as follows:-

	% of total population	% of those living in purpose built flats
Under 16	18.8	10.5
16-29	24.6	32.7
30-44	20.7	24.4
45-64	22.7	18.1
65-74	6.7	6.2
Over 75	6.4	8.0

8.77 These figures indicate that on average fewer children and more young adults and over 75s reside in apartments across the City. However, who eventually occupies the market houses and apartments on this site is unknown. The Housing Officer has indicated that the affordable units would be for the elderly.

8.78 There is an existing variety of housing types within the area catering for a wide demographic range not just the elderly. The mean age of Heath residents is 41 with 17.3% of the population 65 or over.

8.79 In view of the unknowns and relatively small number of future occupants compared to the overall population of the Heath ward it is not considered that this issue is of any great weight in the determination of this application.

Lack of affordable housing

8.80 The provision of affordable housing on the HPP sites exceeds that required under the 20% requirement on brownfield sites specified in the Council's planning policies. In any event this proposal is part of a larger project to provide substantially more affordable housing across the City.

Moneymaking scheme

8.81 The proposal is part of a larger partnership scheme to provide more affordable housing across the City to help towards addressing a pressing social need. The Council's partner is a house builder that operates under normal commercial principles.

An excuse to help the homeless

8.82 The need to provide homes for people in need is not an excuse or pretence. There is considerable unsatisfied demand for affordable housing in Cardiff and this scheme on its own makes a modest contribution to addressing that demand but is part of a much larger scheme to provide substantially more affordable housing across the City. Notwithstanding this each planning application has to be judged on its own planning merits as a residential development. In this case the major element of the scheme is for open market housing.

No lift within the apartments

8.83 There are two affordable units and four open market units to be accommodated on the second floor of the apartments. The occupants of the affordable units will be selected by the Council's housing allocation officer and those choosing to live in the market housing will have to determine whether the accommodation suits their particular needs.

8.84 The Housing Officer has confirmed that DQR only necessitates lifts in 4 storey apartments. The Council's allocations team take various factors into account when allocating properties.

The policies of well-being are ignored.

8.85 The 7 well-being goals are as follows:

- A prosperous Wales
- A resilient Wales
- A healthier Wales
- A more equal Wales
- A Wales of cohesive communities
- A Wales of vibrant culture and thriving Welsh language
- A globally responsible Wales

8.86 Paragraph 3.2 of the LDP vision is as set out in the 10 year, 'What Matters' Strategy (2010-2020) which is that "By 2020...Cardiff will be a world class European capital city with an exceptional quality of life and at the heart of a thriving city-region".

8.87 Paragraph 3.3 states that Partners have agreed seven strategic outcomes that, if achieved would represent ultimate success and the realisation of the vision. The outcomes are that:

People in Cardiff are healthy;
People in Cardiff have a clean, attractive and sustainable environment;
People in Cardiff are safe and feel safe;
Cardiff has a thriving and prosperous economy;
People in Cardiff achieve their full potential;
Cardiff is a great place to live work and play; and
Cardiff is a fair, just and inclusive society.

- 8.88 Paragraph 3.4 states that It is important to recognise that the LDP cannot deliver all of these outcomes alone as many issues extend beyond land use planning matters and the remit of the document. However, the LDP is a crucial strategic document that must create the right conditions which both directly and indirectly assist and support the delivery of these outcomes.
- 8.89 The creation of employment during the construction period and providing safe modern energy efficient dwellings that incorporates affordable housing in a sustainable location close to public transport, parks and a major employer whilst safeguarding the amenities of adjoining residents goes towards meeting the afore mentioned policies and strategic outcomes.
- 8.90 Section 3 of the Well-Being of Future Generations Act 2016 imposes a duty on public bodies to carry out sustainable development in accordance with the sustainable development principle to act in a manner which seeks to ensure that the needs of the present are met without compromising the ability of future generations to meet their own needs. This duty has been considered in the evaluation of this application. It is considered that there would be no significant or unacceptable impact upon the achievement of wellbeing objectives as a result of the recommended decision.

Loss of trees

- 8.91 The SPG on trees says that category A and B trees can be removed if there are overriding design considerations and their loss can be successfully mitigated by new planting. 12 trees (including one A and six B trees) are to be removed and 11 new trees planted. The Tree Officer has carefully assessed the loss of trees and has no objection to the landscaping proposed. The trees on site are afforded no special protection and can be removed without the approval of the Local Planning Authority. Proposed conditions 11, 12 and 13 are considered necessary to secure successful landscaping arrangements.

Who will buy these units and for how much.

- 8.92 How the applicant sells the open market residential units is a commercial matter for the house builder not one controlled by the Local Planning Authority. Who will buy these units is an unknown at this stage and will become clearer to the applicant when properties are marketed but as happens elsewhere dwellings will be resold over time. This issue is not a planning consideration. In due course the Council's Housing Department will allocate tenants for the affordable units.

No minutes of pre-application discussions and application submitted immediately after PAC completed

- 8.93 When the applicant submits a planning application after the end of the PAC process is a matter for the applicant to determine and not the Local Planning Authority.
- 8.94 The applicant engaged with the Planning Department in a pre-application advisory process. However, since this application has been submitted issues have been identified by objectors and officers and further discussions with officers has taken place resulting in amendments that have improved the scheme from that originally submitted. Members know that each application has to be assessed by the Local Planning Authority based on the details accompanying the application, the policies of the LDP, SPGs, and consultation responses and have regard to the comments made by objectors.

No analysis of school requirements

- 8.95 The Education Officer was consulted on this application and has raised no objection.

Housing Association not consulted

- 8.96 There is no planning requirement for the Local Planning Authority to consult with housing associations on planning applications of this nature.

Passivhaus

- 8.97 The Design Commission for Wales has previously expressed support particularly the piloting of Passivhaus for the housing partnership. The DCfW acknowledged the commitment of the applicant to high energy efficiency principles which have shaped the grouping of units. This is unusual in Cardiff and is to be welcomed.

- 8.98 The applicant says his team has been working with Warm an independent consultancy. Passivhaus will contribute to sustainable development. Their advisor says that unlike normal buildings when you open an external door in a Passivhaus there is no rush of air as the fabric is so airtight and therefore a hall is not required. A local resident has further disputed the Passivhaus quality of the scheme and considers this cannot justify this development. The amended house type was submitted after the developer had liaised with Warm in respect of the Passivhaus requirements.

- 8.99 An energy efficient scheme is supported by the DCfW, national and LDP policies, however, the technical control for the construction of buildings to meet energy efficient standards lies under the Building Regulations. The applicant considers the Passivhaus approach is material to the determination of this application. Whilst Passivhaus is a consideration there are several other significant considerations in the determination of this application which are addressed elsewhere within this section of the report.

Inadequate Publicity

- 8.100 Concern has been raised over the publicity of this application. Neighbour notification letters were sent to adjoining owner/occupiers in line with government advice and also residents on the other side of Allensbank Road and Heathmead that would face the site. In addition 4 site notices were erected to the north, west, south and east of the site. The application was also advertised in the Western Mail in line with normal practice and has been publicised on the Council website.

- 8.101 The neighbour notification letter encourages objectors to access the Council website, which is more convenient than visiting County Hall, and to respond electronically, which is more efficient. The neighbour notification letter contains the Council's address should an objector wish to post an objection. The site notice encourages the use of the Council website to respond but gives an alternative option to respond with the Council's postal address. The Press Notice dated 26/10/17 says that the application details can be inspected at County Hall during normal office hours. The 14 day notification period for the amendments is normal practice having regard to the nature of the amendments (access arrangements, appearance of terraced units, landscaping, boundary detailing at junction of Allensbank Road/Heathmead, an additional visitor parking space, reposition bin/cycle store, size of balconies, and side windows to south elevations of apartments). The notification letter was sent out on 11/01/2018.
- 8.102 The publicity undertaken exceeds that required under the Town and Country Planning (Development Management Procedure)(Wales)Order 2012. The requirements for a major application are site notice or neighbour notification letter, newspaper advertisement and website.
- 8.103 The other notice referred to is in respect of the pre application consultation publicity carried out by the developer, which was not a matter for the Local Planning Authority to conduct.
- 8.104 In view of the extent of the responses received it would appear that the Local Planning Authority's publicity exercise has been reasonably effective.

Other Issues

- 8.105 Paragraph 4.20 of the LDP says that brownfield and windfall sites continue to play an important role in providing new homes for the city to continue to evolve. This equates to approximately 12% of Cardiff's housing needs over the LDP period. The 42 dwellings proposed in this application forms a small part of that 12%.
- 8.106 The LDP forms the basis for determining planning applications. The Council and Planning Inspector took account of PPW, the Tans and other government advice at the Public Inquiry that approved the LDP. The relevant LDP policies of the Authority have been referred to earlier in this report and this development is considered to be in accordance with those policies.
- 8.107 Planning Policy Wales at paragraph 3.1.4 states:
Factors to be taken into account in making planning decisions (material considerations) must be planning matters; that is, they must be relevant to the regulation of the development and use of land in the public interest, towards the goal of sustainability (see 4.2). The planning system does not exist to protect the private interests of one person against the activities of another. Proposals should be considered in terms of their effect on the amenity and existing use of land and buildings in the public interest. When determining planning applications local planning authorities must take into account any relevant view on planning matters expressed by neighbouring occupiers, local residents and any other third parties. While the substance of local views must be considered, the duty is to decide each case on its planning merits.

- 8.108 The applicant originally proposed a section of timber fencing fronting Heathmead at the end of the southernmost terraced unit. Following discussions with officers the boundary wall and hedge will now be retained which will still safeguard the privacy of future occupants but improves the appearance of the corner from that originally submitted.
- 8.109 The landscape bordering the Heathmead boundary forms part of the planning application, and includes the repositioning of a substation. Whilst there would still be some planting around the new substation it is likely that new planting will be required to help integrate it into the street scene. Condition 13 seeks to address this.
- 8.110 In respect of noise the gardens are located on the screen side of the dwellings and the submitted plans specify 1.8m high walls to the rears of the dwellings and a 1.8m high close boarded fence behind the hedge and existing boundary wall to the southernmost dwelling. Pollution control has no objection on grounds of noise.
- 8.111 Both the Regeneration Officer and Parks Officer are seeking S106 contributions totalling £116,360. Following these requests a viability appraisal by GVA was commissioned by the Housing Department. GVA state that the proposed scheme does not provide an appropriate return to either the land owner or developer. The appraisal concludes that increasing costs by requiring a S106 contribution will mean the scheme becomes less viable and that the viability of the scheme should not be prejudiced by an unrealistic S106 requirement. The viability appraisal has been referred to Estates Management for a review and the numbers in the appraisal have not been disputed.

Amendments

- 8.112 Most of the amendments received are relatively modest in nature. However, the amendments to the front elevations of the houses have made a significant improvement to their appearance when viewed from the public highway.
- 8.113 In respect of the new issues raised by objectors I consider the following to be relevant:

(a) The Transportation Officer states:-

Whilst agreeing with the objector that TD 42/95 Geometric Design of Major/Minor Priority Junctions provides guidance on junction layout this document is written specifically for the trunk road network. As advised by Manual for Streets 2 all DMRB guidance should not be applied uncritically in other environments (para. 9.4.2).

The visibility splay applied in the application would have used an X distance of 2.4m, rather than 4.5m, in accordance with advice within Manual for Streets (para 7.7.8) and Manual for Streets 2, which states (para 10.5.6) "An X distance of 2.4m should normally be used in most built-up situations, as this represents a reasonable maximum distance between the front of a car and the driver's eye." In this instance application of the 2.4m X distance is considered the more appropriate approach to assessing visibility. It is also worth considering that Manual for Streets also states that "Longer X distances may also result in more shunt accidents" (para 7.7.9).

The Y distance applied in the application and accepted by the highway authority, given the nature of Allensbank Road, as advised in Manual for Streets (table 7.1) would be 43m as detailed in the Transport Statement.

The proposed radius has been accepted at this stage, but it should be considered that the detailed design and layout of the junction including the radius would be conditioned and scrutinised by the council's specialist design team as part of the Section 278 process.

(b) The Transportation Officer has confirmed that refuse vehicles can manoeuvre within the proposed development.

8.114 *Equality Act 2010* – The Equality Act 2010 identifies a number of 'protected characteristics', namely age; disability; gender reassignment; pregnancy and maternity; race; religion or belief; sex; sexual orientation; marriage and civil partnership. The Council's duty under the above Act has been given due consideration in the determination of this application. It is considered that the proposed development does not have any significant implications for, or effect on, persons who share a protected characteristic in the vicinity of the site or who may occupy the proposed dwellings.

8.115 The Affordable Housing Officer, Wales and West Utilities, Contaminated Land Team, Fire and Rescue, NRW, GGAT, South Wales Police, Tree Officer, Pollution Control, Welsh Water, Parks Officer, Transportation Officer, Waste Management Officer, Air Quality Team, Noise Team, Regeneration Officer and Education Officer have raised no objection to the development

8.116 Notwithstanding the various and strongly held objections/concerns raised by two local members, MP, two AMs, Heath Residents Association and residents in the surrounding area the proposal complies with the Council's LDP policies and SPG on residential amenity and car parking standards. The proposal makes efficient use of land in a safe, well-designed scheme that positively contributes to the provision of affordable housing and increases housing supply and choice within the City in a sustainable location on a brownfield site.



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Project
HIGHFIELDS, HEATH

Client
WATES RESIDENTIAL

Drawing Title

SITE LOCATION PLAN

Date
JUN '17

Scale
1:1250 @ A4

Drawing No.
3 5 1 1 PA 0 0 1

Rev.
-

NOTES. Do not scale. All dimensions are in millimetres unless stated otherwise



20 m
100 ft

Revisions	Date
A Apartments 33 - 36 layouts amended.	22 SEP '17
B Brickwork wall to rear of houses.	03 OCT '17
C Access to Heathmead removed and footpath relocated. Access to Allensbank Road widened and landscape buffer removed. Bike store relocated and visitor space added. Paths aligning primary access now lead directly across shared surface. Boundary to plot 1 amended. Gateway features removed. Windows to gable ends of house enlarged. Locations of new garden centre signs indicated.	17 NOV '17
D Bell-mouth junction added. Tree added adjacent to apartments central entrance. Trees to rear of houses removed. Errors in schedule corrected.	27 NOV '17
E Internal house layouts amended, front walls and landscaping adjusted in accordance with amendments.	10 JAN '18



KEY	TENURE	APARTMENT TYPE	AREA	NO. (S)	NO. (AFF)	TOTAL
(H)	SALE	2 BED 4 PERSON	72.5m ²	12	0	12
(A1)	SALE	2 BED 3 PERSON	61m ²	8	0	8
(A2)	SALE	2 BED 3 PERSON	63m ²	8	0	8
(A3)	SALE	2 BED 3 PERSON	61m ²	3	0	3
(A4)	SALE	2 BED 3 PERSON	61m ²	1	0	1
(A5)	AFFORDABLE	1 BED 2 PERSON	53m ²	0	2	2
(A6)	AFFORDABLE	1 BED 2 PERSON	56m ²	0	2	2
(A7)	AFFORDABLE	2 BED 3 PERSON	64m ²	0	2	2
(A8)	AFFORDABLE	2 BED 3 PERSON	62m ²	0	2	2
(A9)	AFFORDABLE	1 BED 2 PERSON	53m ²	0	1	1
(A10)	AFFORDABLE	2 BED 3 PERSON	62m ²	0	1	1
TOTAL				32	10	42

BOUNDARY TYPES
Application boundary.
0.9m high stone wall (hatching indicates where wall requires infilling).
0.9m tall black ppc metal railings and gates.
1.8m brickwork wall. Brick on edge capping (gates within to match height of wall).
2.1m brickwork wall boundary to public open spaces (gates within to match height of wall).
1.8m brickwork wall boundary to public open spaces (gates within to match height of wall).
1.8m close boarded fence / gates.
1.8m close boarded fence above retaining structure.

LANDSCAPING MATERIALS
Flag Paving - Natural.
Block Paving (Light) * Indicates rumble strip
Block Paving (Medium)
Block Paving (Dark)
Block Paving (Dark)
Tarmac
Low maintenance, shrub planting. Approx. 0.7m.
Proposed hedge planting (1m tall).

SYMBOLS
Plot numbers
Indicates Affordable Dwelling
Car parking space allocation.
Rotary dryer.

North

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Project: HIGHFIELDS, HEATH
Client: WATES RESIDENTIAL
Drawing Title: COLOURED SITE LAYOUT PLAN

Date: JUN '17
Scale: 1:200 @ A1
Drawing No: 3511 PA 003
Rev: E

NOTES: Do not scale. All dimensions are in millimetres unless stated otherwise

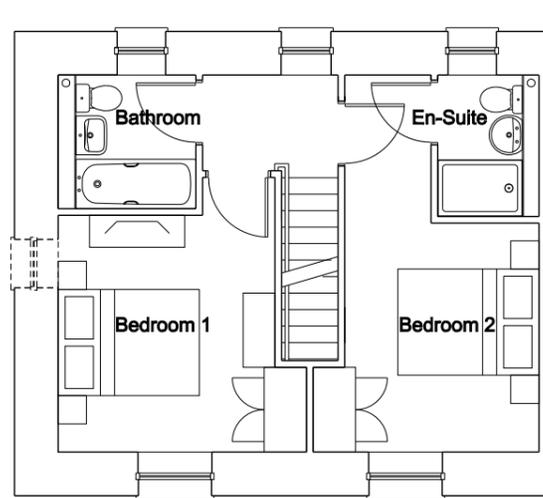
Revisions

A INCREASED BRICK SURROUND TO FRONT DOOR & STAIR WINDOW, CHIMNEYS ADDED, DOWNPIPE RE-LOCATED.

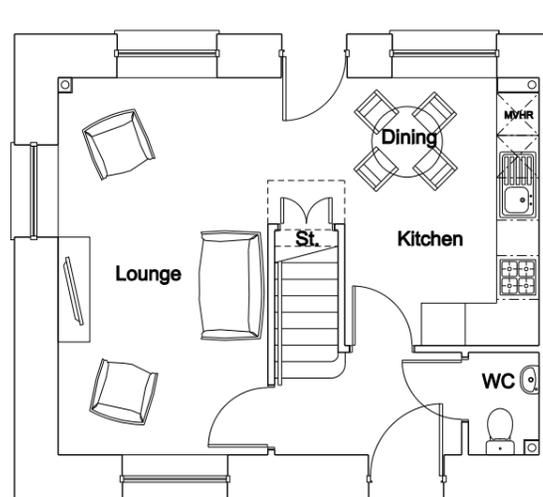
17 NOV '17

B INTERNAL LAYOUT RECONFIGURED, ELEVATIONS UPDATED.

10 JAN '17



First Floor
1:100



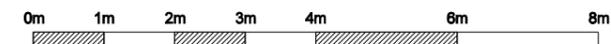
Ground Floor
1:100



Front Elevation
1:100



Rear Elevation
1:100



Finishes:

Walls:
1bstock red multi gilt stock brick plinth with natural mortar below roughcast render finish.

Pitched roofs:
Marley Ashmore 'old english dark red' interlocking concrete tile with mortar bedded verges.

Windows and doors:
High performance triple glazed windows. Frame in RAL7001 Silver Grey Finish. Solid front and rear doors.

○ Denotes opaque glazing.

Window sills:
Pressed aluminium sills in Mid Grey (RAL 7001).

Fascias:
Black PVCu

Rainwater goods:
Black PVCu

N.B. Location of all service inlet/extract terminals & boiler flues to be confirmed, following design by specialist manufacturer.

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Client
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Drawing Title

**HOUSE GENERAL ARRANGEMENT
AND ELEVATIONS**

NOTES. Do not scale. All dimensions are in millimetres unless stated otherwise

GIA: 72.5m²

Date
JUN '17

Scale
1:100 @ A3

Drawing No.
3511_PA_015

Rev.
B



Revisions

- A GARDEN WALL CHANGED TO BRICK 22 SEP '17
- B BRICK DETAILING TO WINDOWS AND DOORS INCREASED. CHIMNEYS AND GABLE WINDOW ADDED. GATEWAY FEATURES AND PLANTING AREA REMOVED. 17 NOV '17
- C STREET SCENE UPDATED IN RELATION TO AMENDMENTS TO INTERNAL PLANS AND EXTERNAL LANDSCAPING 15 JAN '18

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Project
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Client
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Drawing Title

VIEW FROM ALLENSBANK ROAD

NOTES. Do not scale. All dimensions are in millimetres unless stated otherwise

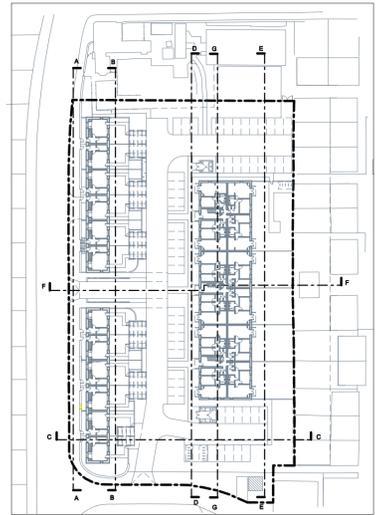
Date
JUL '17

Scale
NTS @ A3

Drawing No.
3511_PA_030

Rev.
C

- Revisions
- A INCREASED BRICK SURROUND TO FRONT DOOR & STAIR WINDOW, CHIMNEYS ADDED, DOWNPIPE RE-LOCATED TO HOUSES. SOUTH ELEVATION APARTMENT WINDOWS AMENDED. 17 NOV '17
 - B HOUSE ELEVATIONS UPDATED TO REFLECT REDESIGN OF INTERNAL LAYOUT. 17 NOV '17
 - C BRICK BLOCKS MARKING ENTRANCE TO REAR RESIDENTIAL ROAD REMOVED. 16 JAN '18



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Project
HIGHFIELDS, HEATH

Client
WATES RESIDENTIAL

Drawing Title
ELEVATIONS AND SECTIONS 1

Date
JUN '17

Scale
1:200 @ A1

Drawing No.
3511 PA 020

Rev.
C

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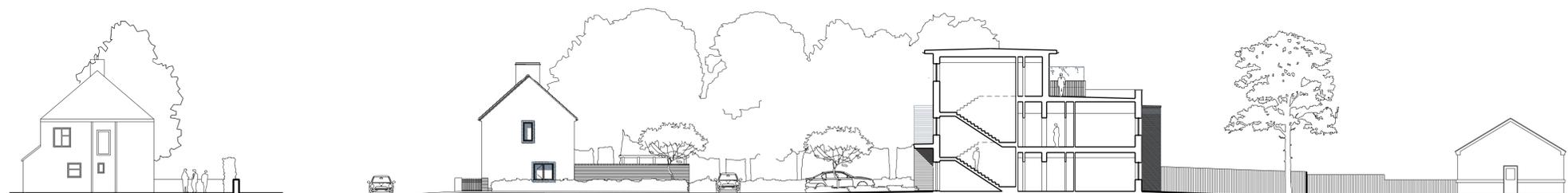
Revisions
 A Apartments 33 - 42 window and balcony positions adjusted. 22 SEP '17
 B Brick rear wall to houses. 03 OCT '17
 C Windows added to house gable and balcony widths increased to apartments. 17 NOV '17



Section D



Section E



Section F



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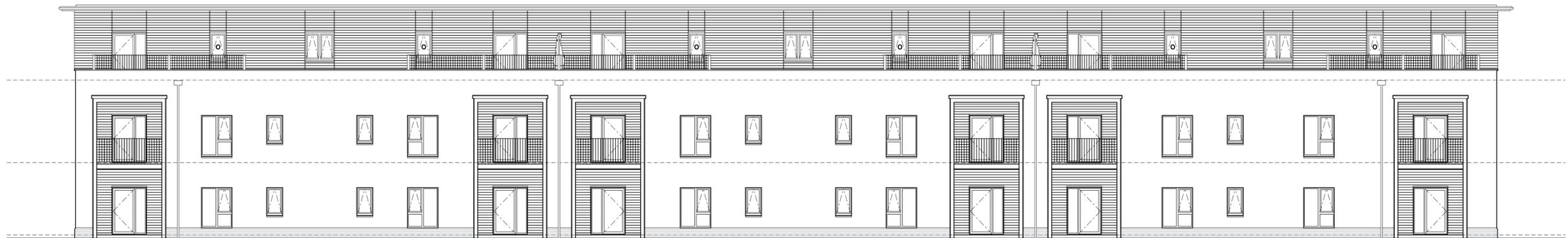
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Project	HIGHFIELDS, HEATH	Date	JUN '17
Client	WATES RESIDENTIAL	Scale	1:200 @ A1
Drawing Title	ELEVATIONS AND SECTIONS 2	Drawing No.	3511 PA 021
		Rev.	C

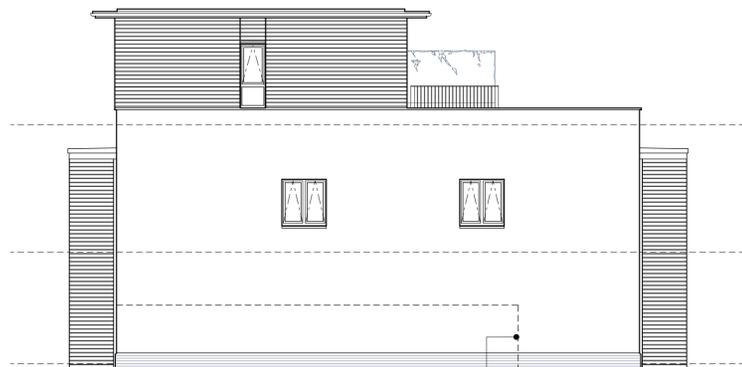
NOTES: Do not scale. All dimensions are in millimetres unless stated otherwise



West Elevation

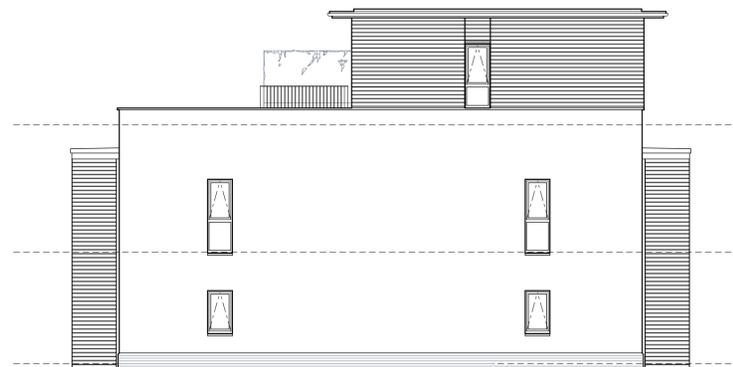


East Elevation



South Elevation

Notional position of bin and bike store in relation to elevation.



North Elevation

FINISHES

Facing Brickwork:
Blended Red Multi Gilt Stock with plinth stretcher brick to top course.

Mortar:
Remix Natural Light.

Render:
Off-white in scraped finish.

Timber Cladding:
Horizontal tongue and groove cladding. Vertical formers adjacent to openings (second floor only).

Balconies:
Horizontal square edge cladding (with regular spaces between boards). Galvanised steel frames with powder coated pressed metal to front face (mid-grey).

Windows and doors:
Triple glazed windows and doors in Silver Grey finish RAL 7001

Flat Roof:
Liquid GRP in dark grey colour with metal edge trim. Horizontal tongue and groove cladding to soffit with square-edge cladding board to eaves.

Window sills, parapets and metal trims:
Mid grey coated pressed metal.

Roof terrace and balcony guarding:
Mid grey powder coated galvanised steel

Rainwater goods:
Black PVC.

N.B. Location of all service inlet/extract terminals & boiler flues to be confirmed, following design by specialist manufacturer.

O Denotes opaque glazing.



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Project
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Client
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Drawing Title
APARTMENTS ELEVATIONS

Date
JUN '17

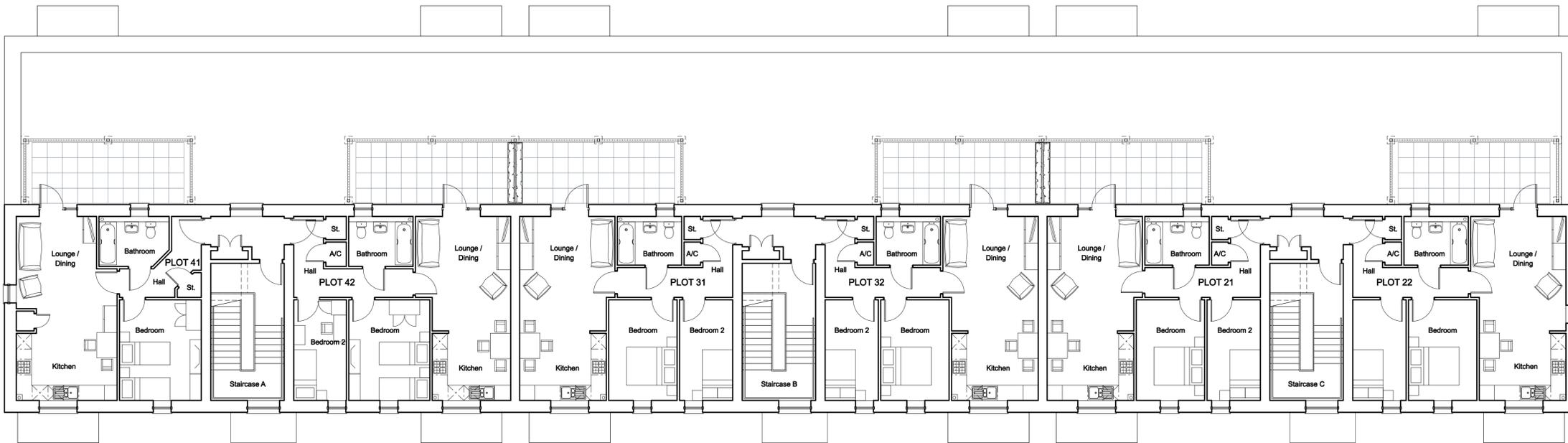
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Drawing No.
3511 PA 011

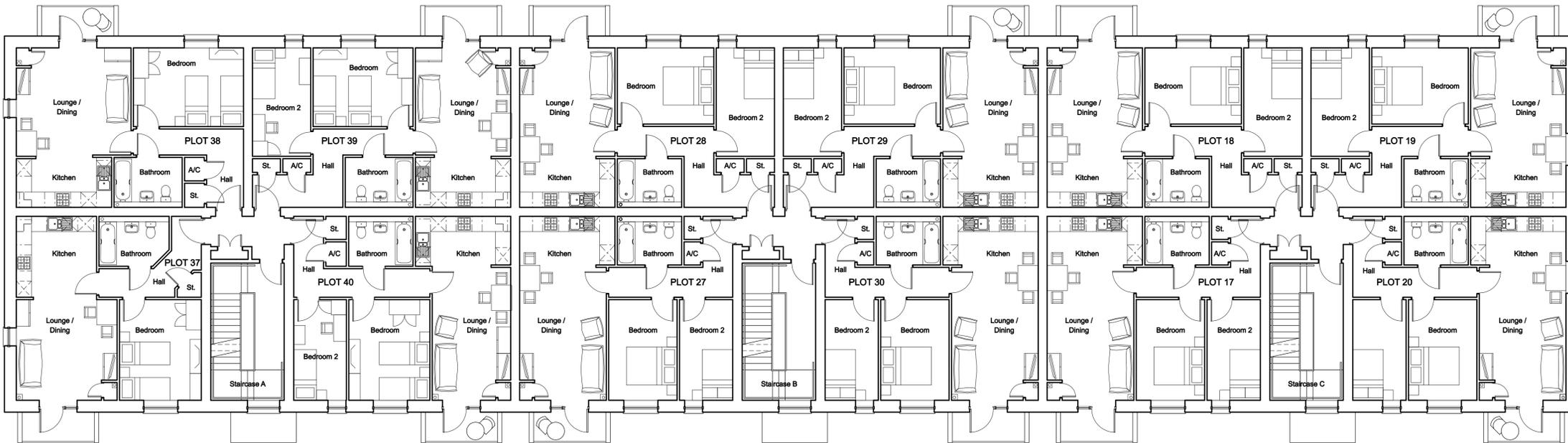
Rev.
C

NOTES: Do not scale. All dimensions are in millimetres unless stated otherwise

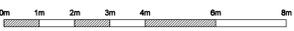
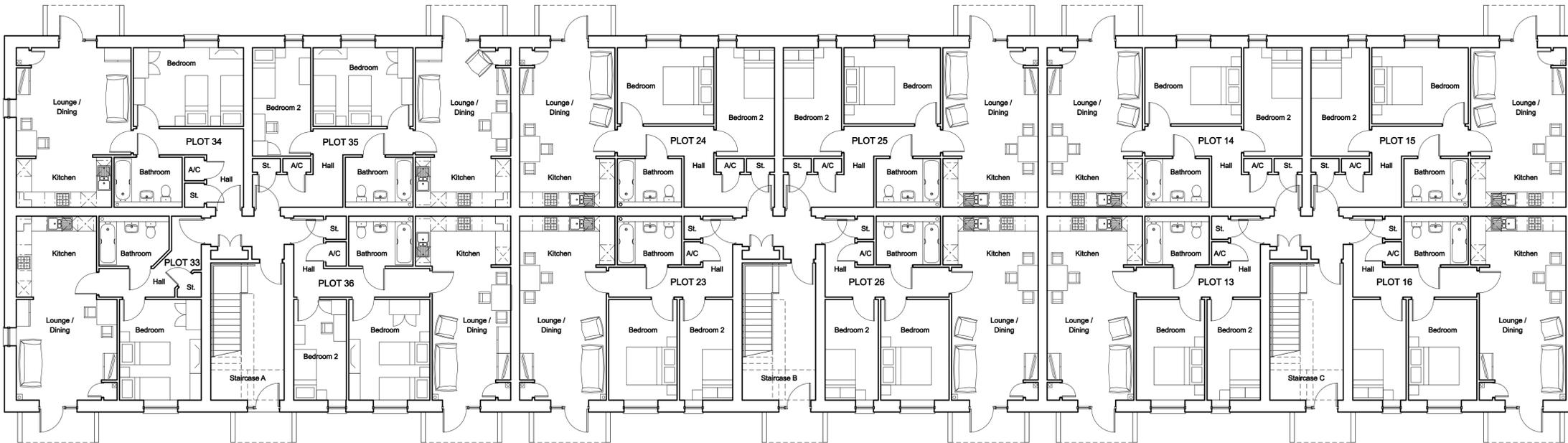
Second Floor Plan



First Floor Plan



Ground Floor Plan



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Project
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Client
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Drawing Title

**APARTMENTS GENERAL
ARRANGEMENT**

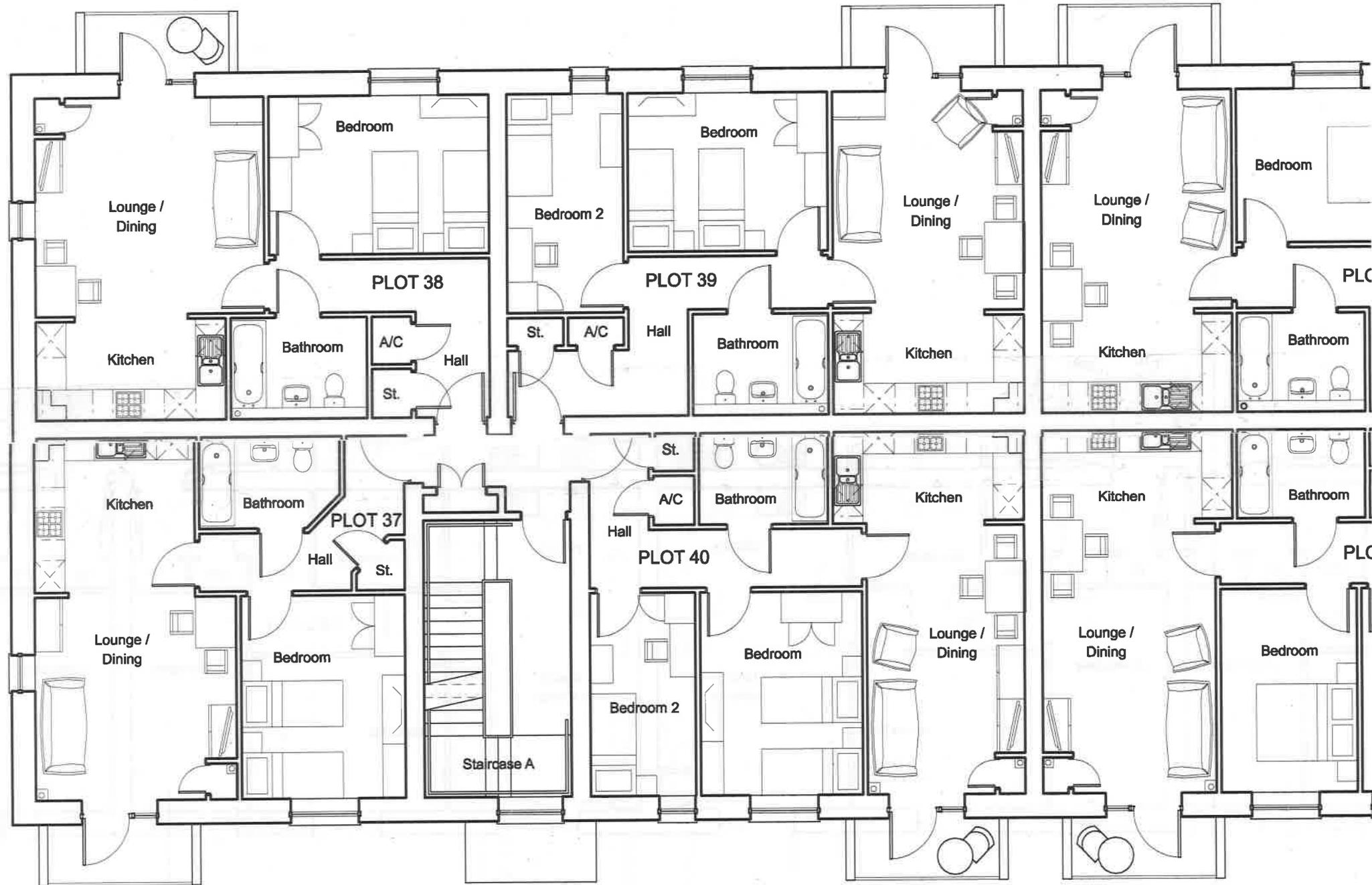
NOTES: Do not scale. All dimensions are in millimetres unless stated otherwise

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JUN '17
Scale
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Drawing No.
3511 PA 010
Rev.
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Affordable Entrance

Open Market Sales Entrance

Open Market Sales Entrance







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Drawing Title

VIEW FROM HEATHMEAD

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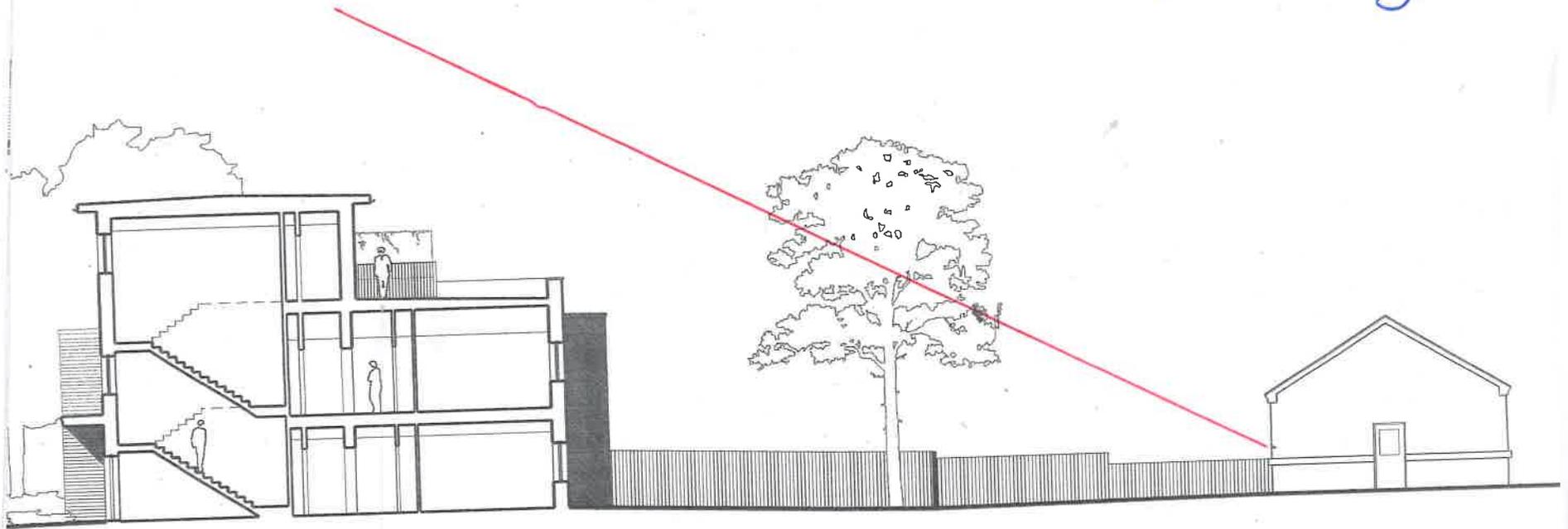
Date
JUL '17

Scale
1:100 @ A3

Drawing No.
3511 PA 032

Rev.
-

25° angle



12/00213/DCO



STRATEGIC ESTATES



ACCESS

Draft Layout Option 1

Oaks garden nursery and associated parking spaces retained

Accessible thresholds to be provided to dwellings

Vehicular access is widened to allow access for the garden nursery and new properties. One way vehicular entrance maintains existing arrangements. Exit to the south onto Heathmead. Easy for refuse and delivery vehicles to service the dwellings.

Convenient and overlooked car parking provision

Access to utilities is retained

Retained pedestrian access aids permeability

To create vehicular access for parking and allow overlooking of the street, the vegetated verges would need to be removed and the pavement realigned in consultation with responsible service areas (ownership: Strategic Estates – non operational land).

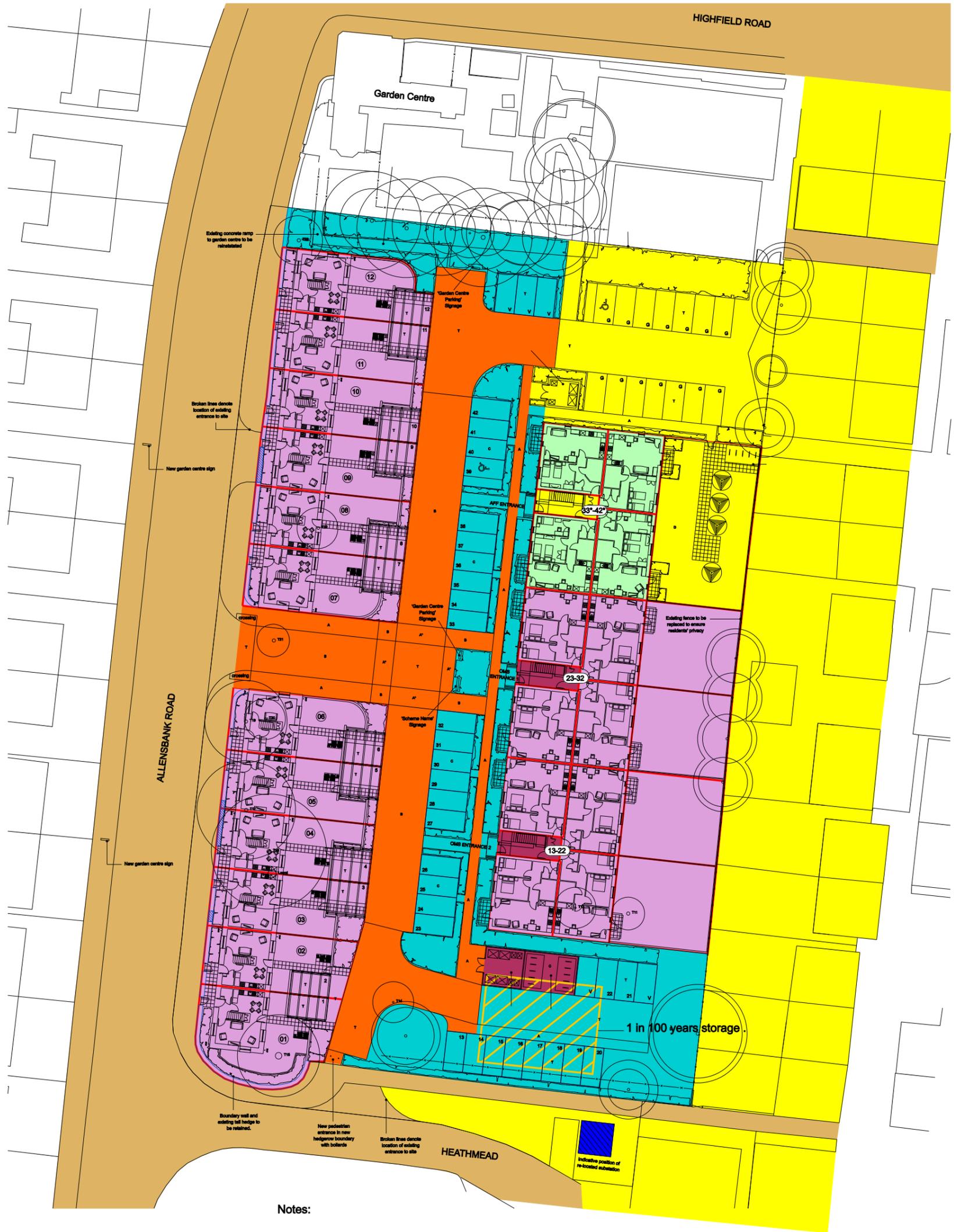


Clear, safe and level pedestrian footways

Pedestrian route through to neighbouring development is retained



Revisions		
B	SUDS note updated.	03 OCT '17
C	Updates to shared surface and entrance to Heathmead omitted.	17 NOV '17
D	Updated to reflect site layout changes	10 JAN '18



Notes:

- All parking to be allocated including affordable flats and maintained/enforced by ROMC.
- Window cleaning to sales flats be undertaken by ROMC.
- Window cleaning to affordable flats to be undertaken by CCC.



- Demise of plot
- Existing adopted highway
- New adopted highway
- Maintained by affordable tenant
- Maintained by CCC Housing
- Maintained by private owner
- Maintained by Resident Owned Management Company (Estate costs)
- Maintained by Resident Owned Management Company (Costs shared between OMS apartments)
- Maintained by WPD
- SUDS to be maintained by Resident Owned Management Company.

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Project
HIGHFIELDS, HEATH

Client
WATES RESIDENTIAL

Drawing Title
MANAGEMENT PLAN

NOTES. Do not scale. All dimensions are in millimetres unless stated otherwise

Date
JUN '17

Scale
1:500 @ A3

Drawing No.
3511 PA 005

Rev.
D